

Submission to Draft Louth County Development Plan 2021-2027

December 2020



Prepared on behalf of
Doyle Shipping Group


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Document Control Sheet

Client	Doyle Shipping Group		
Project Title	Draft Louth County Development Plan 2021-2027		
Document Title	Submission		
Document No.	4226		
Prepared by	KK	Checked by	PG

Rev.	Status	Issue Date
A	Final	23.12.20

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1.0 Introduction

McCutcheon Halley Chartered Planning Consultants of Kreston House, Arran Court, Arran Quay, Dublin 7, wish to make a submission on the Draft Louth County Development Plan 2021-2027, on behalf of our Client, Doyle Shipping Group (DSG). Our Client, is a key stakeholder in the Development Plan making process, being the owner of a significant infrastructural asset, Greenore Port.

The County Development Plan is key to the future development of ports. As an island economy, there are limited alternatives available to the use of sea transport for the movement of freight and bulk commodities. Air freight is constrained by the volumes that can practically be carried by air, by cost and environmental disadvantages. As a consequence, shipping will continue to provide the only effective way to move the vast majority of freight in and out of the State, and the provision of sufficient sea port capacity will remain an essential element in ensuring sustainable growth in the Irish economy.

Greenore Port is the only deep-water port on the east coast outside of Dublin Port. It provides a range of services including general cargo handling, pilotage and customs. The main commodities handled are bulk animal feed, fertiliser, steel, timber and general cargo. There are 2 no. berths - the outer berth (Berth 1), which accommodates vessels up to 200m, and the inner berth (Berth 2), which accommodates vessels up to 110m. The port can handle ships of up to 200m length, 32m beam, 8.5m draught and 60,000 tonnes deadweight.

The port's infrastructure was largely outdated and not fit for purpose when it was acquired by DSG in 2014. To secure its future, DSG have implemented a programme of improvement works including investment in warehousing, offices, dredging and storage areas. By June 2021, and on completion of the refurbishment of Berth 2, DSG will have invested approx. €20 million in Greenore Port.

DSG are committed to transparency regarding their plans for the future sustainable growth of the port. In this regard, a **Vision Document** has been published and presented to Louth County Council and local residents. It sets out the envisaged future development of the port. Notably, unlike all other ports in the State, Greenore Port is privately owned and their investment horizon is much shorter than State owner ports as they are reliant on securing funding from private investors. This limits their ability to project beyond a 5-year horizon. However, the benefit is that the Vision is a realistic picture of what may be achieved. This document has been presented to Louth County Council and the residents of Greenore village.

Our Client is committed to their responsibilities as stewards of the Protected Structures located within the port's landholding. In 2019, DSG commissioned the preparation of a **Built Heritage Strategy** to get professional guidance for the protection and maintenance of heritage assets within the port's landholding. This strategy was issued to Louth County Council. On foot of this, a programme of works to the Lighthouse complex are nearing completion.

To ensure the continued success of Greenore Port, it is critical that an appropriate policy framework is integrated to the forthcoming Development Plan to stimulate investment confidence in long-term infrastructure projects.

The purpose of this submission is to highlight DSGs future plans for Greenore Port to Louth County Council so that a robust and informed policy environment can be formulated to support its contribution to the County, Eastern and Midlands Region and the State.

2.0 Policy Context

It is necessary to consider all inter related policy when formulating the forthcoming Louth County Development Plan. Set out below are national, regional and external policies that are relevant to the development of ports in County Louth and that must be given consideration in the plan making process.

2.1 National Ports Policy 2013

The National Ports Policy (NPP) 2013 establishes a framework for the future sustainable development of commercial ports in the State. The core objective of the NPP is to facilitate a competitive and effective market for maritime transport services.

The NPP introduced a categorisation of the ports sector , Ports of National Significance (Tier 1 and Tier 2) and Ports of Regional Significance. All ports (Greenore, Drogheda and Dundalk) in County Louth are assigned Regional Significance.

Ports of Regional Significance are considered within the NPP to function as important facilitators of trade for their regional and local hinterland. However, ports within this category are considered to be *“limited in their future potential as centres of commercial shipping”*. This statement gives rise to uncertainty in terms of securing investment for the future growth and development of Greenore Port.

Greenore Port handles much larger vessels compared to other ports of regional significance. In comparison with some of the listed Tier 2 ports, e.g. Rosslare and Waterford, Greenore has superior infrastructure available, such as stevedoring facilities. In addition, the port which is handling the most tonnage out of the ‘ports of regional significance’ is Drogheda, the largest vessel they can cater for is approx. 120m in length, while Greenore can accommodate vessels of 200m.

Although Greenore Port handles less tonnage to Waterford (Tier 2), the port has the capacity to handle bigger ships compared to the 190m vessels that Waterford can handle.

Upon completion of Berth 2 in Q3 2021, Greenore Port will be able to accept vessels of 225m in length and 75,000T deadweight. The only ports in the Republic of Ireland capable of this will be Port os National Significance (Tier 1) Dublin, Cork, Foynes and Greenore.

Three criteria are established within the NPP to qualify as a Tier 2 Port and they are:

1. responsible for at least 2.5% of overall tonnage through Irish ports;
2. have the clear, demonstrable potential to handle higher volumes of unitised traffic, and
3. have the existing transport links to serve a wider, national marketplace beyond their immediate region.

In 2019, Greenore Port handled approx.1.9% of the State’s overall tonnage. Since acquiring the port in 2014, the throughput has increased by 55%, from 502,304 tonnes in 2014 to 1,105,000 in 2020. On completion of the Berth 2 improvement works together with other planned infrastructure development, Greenore Port will have the capacity to handle greater volumes. Greenore is located east of the Dublin-Belfast corridor, some 15kms east of the M1 motorway, making the Port ideally situated to serve a wide area. Indeed, existing goods arriving and departing from Greenore serve a wide geographical area that extends far beyond the immediate area.

Should Greenore Port continue to grow as it has done over the last 5 years and receive endorsement through robust Development Plan policies, it is anticipated that it would meet the criteria for Ports of National Significance, Tier 2, designation by 2025.

It should be noted that National Ports Policy 2013, which competent authorities must have regard to in making decision on port development proposals, is not strictly applicable to Greenore Port. As Ireland’s only privately

owned and operated port, it is even more critical that the Draft Plan provides for a policy objective specific to Greenore Port, which can support its continued growth as a robust port facility in Ireland.

2.2 National Planning Framework & National Development Plan

A fundamental objective of the NPF is to further support Ireland's high-quality international connectivity which is critical for overall international competitiveness and addressing opportunities and challenges from Brexit through investment in ports and airports.

The development of Ireland's ports is addressed in the National Planning Framework by National Policy Objective (NPO 40): *"Ensure that the strategic development requirements of Tier 1 and Tier 2 Ports, ports of regional significance and smaller harbours are addressed as part of Regional Spatial and Economic Strategies, metropolitan area and city/county development plans, to ensure the effective growth and sustainable development of the city regions and regional and rural areas"*

The National Development Plan recognises that Ireland must be capable of delivering additional port capacity in a timely and predictable manner. This builds on the core objective of National Ports Policy to facilitate a competitive and effective market for maritime transport services.

Greenore Port has a key role to play in delivering on NPO 40. It is assigned regional significance but is advancing toward meeting the criteria of Tier 2 ports. Its natural deep water coupled with its ability to grow in a timely manner due to the availability of undeveloped land means that it can upscale relatively quickly.

2.3 Eastern and Midlands Regional Economic and Spatial Strategy (RSES)

County Louth is within the jurisdiction of the Eastern and Midlands Regional Assembly area. The RSES provide a framework for the development of the region over the next 20 years.

The principal objective is to foster, structure and focus development potential, to build a competitive and attractive region. The RSES recognise that the Border region provides an important interface between two national economies of Ireland and North Ireland, possessing key development linkages and opportunities.

The RSES identifies Economic Opportunity as one of its key principles;

"To create the right conditions and opportunities for the Region to realise sustainable economic growth and quality jobs that ensure a good living standard for all."

The Dublin – Belfast Economic Corridor is recognised as the largest economic agglomeration in the State and it is noted that the Corridor is the national entry point to the island through its airports and ports. County Louth is highlighted as being strategically located in proximity to the Dublin – Belfast cross-border network, and will become more important post Brexit, acting as national entry point to the island through its port.

The RSES recognises the importance of regional ports such as Greenore and seek to support and enhance their roles as regional economic drivers. Relevant RSES regional policy objectives for ports include;

RPO 8.22 – The EMRA supports ports of Regional Significance that serve an important regional purpose and/or specialised trades or maritime tourism; and the accessibility requirements of regional ports within the Region, from within their regional catchments will be addressed through the provision of improved access routes, where necessary and improved access to the national and regional road networks. Opportunities for the use of rail, where ports are connected to the rail network is also promoted.

RPO 8.23 – The EMRA supports the protection of the marine related function of ports in the Region in order to ensure the future role of ports as strategic marine related assets is protected from inappropriate uses, whilst supporting complimentary economic uses including the potential for facilitating offshore renewable energy developments at ports.

2.4 National Marine Planning Framework

Given Ireland's reliance on maritime transport and inability to switch to alternative transport modes, protecting port development and capacity is a key priority of marine spatial planning.

The Framework highlights that integration and alignment is needed between terrestrial and marine planning processes. Terrestrial planning should coordinate with and support ports with the necessary transport links and suitable road networks.

Chapter 14 of the Framework relates to Ports, Harbours and Shipping and establishes the following Objectives:

- Safeguard the operation of ports as key actors in the economic wellbeing of the State through the provision of safe and sustainable maritime transport.
- Facilitate a competitive and effective market for maritime transport services.
- Sustainable development of the ports sector and full realisation of the National Ports Policy with a view to providing adequate capacity to meet present and future demand, and to adapt to the consequences of climate change.

2.5 National Economic Policy

Enterprise 2025 was published in November 2015 by the Department of Jobs, Enterprise & Innovation. The document sets out the strategic framework for coherence across government departments to deliver sustainable enterprise and growth. Enterprise 2025 notes that, as a small open economy, sustainable long term growth is dependent on continued success in international markets. It notes that:

“An export led strategy is not just about exporters. In addition to generating greater wealth than would be possible through only domestically traded activities, there is a substantial ripple effect in terms of job creation across the entire economy and an increase in technology adoption, innovation, standards and best practice overall.”

To facilitate continued growth in exports, Enterprise 2025 recognises that investment is required in key infrastructure and commits to putting in place the appropriate policy framework to stimulate investment confidence in long-term infrastructure projects. In relation to the delivery of sustainable, reliable and quality transport networks the report includes the following objective:

“In sea transport: Implement the 2013 Ports Policy framework such that the future needs of maritime connectivity are periodically reviewed; that access to a wide range and frequency of port services at competitive prices is ensured; and that deeper water facilities are provided at Irish ports that can accommodate the international trend towards larger shipping vessels.”

Greenore Port is the only deep-water port on the east coast of Ireland, outside of Dublin Port. A shift by global shipping lines toward larger vessels requiring access to deeper water and the reduced availability of vessels to use smaller ports is a challenge which faces the port sector.

2.6 Foodwise 2025

Food Wise 2025 published in 2015 is a growth vision for the Irish agricultural economy and contains 4 headline opportunities:

- increase the value of agri food exports by 85% to €19 billion;
- increase value added to the sector by 70% to €13 billion;
- increase the value of primary production by 65% to €10 billion; and
- In turn, achieving these targets is expected to deliver a further 23,000 jobs in the agri food sector by 2025.

These growth opportunities are forecasted as arising mainly from three changes in the economic environment for the agri-food sector, namely the abolition of milk quotas, strong demand for protein (livestock products and seafood) arising from growing economic prosperity in emerging markets and Africa, and more differentiated consumer demand opening high value added opportunities in consumer markets. Within tillage, the prospects for growth suggest a switch from commodity animal feed grade production to premium production such as malt barley, milling wheat, breakfast cereal production and health food. To meet the fiscal targets set-out in Food Wise 2025 increased volumes of imported animal feedstuff will be required to meet demands of the livestock and dairy sector.

2.7 Renewable Energy

Ireland has ambitious climate targets towards 2030, including a target to develop at least 3.5 GW of offshore wind energy, as published in the Climate Action Plan (CAP) in June 2019. Ports play a significant role in the lifecycle of an offshore wind farm. Installation and decommissioning activities require large ports with the capability of handling large vessels and an extensive supply chain. Whereas; small ports can support daily operations and maintenance activities. The need for a reliable and efficient ports which encompass all the necessary physical characteristics to support the offshore wind activity is therefore critical.

Offshore wind projects aid the regeneration of regional economies. Given the scale of investment required to develop the projects, the regional employment and multiplier effect to supply chains and local communities is very significant. The Sustainable Energy Authority of Ireland have estimated that onshore and offshore wind generation could create over 20,000 direct installation and operation and maintenance jobs by 2040. These jobs would be predominantly created in areas of Ireland with low employment density, such as the western seaboard and the east coast outside the greater Dublin area.

There are three key requirements that enable a port to support offshore wind activities:

- i. Physical Characteristics - berth availability, port depth, quay load bearing capacity
- ii. Location - proximity to potential offshore wind farm locations,
- iii. Port Layout - storage availability, component fabrication ability.

Greenore Port is strategically positioned close to planned offshore wind farm developments in the Irish Sea, the Oriel and Clogherhead Windfarms. Future development and expansion of the port could offer facilities and support to service the marine renewable energy industry.

2.8 Brexit

Brexit has brought into renewed focus the importance of Ireland's ports as nodes in the logistics chain and in keeping Ireland connected internationally.

As an island economy, Ireland is highly dependent on maritime transport. Trading as it does in the global marketplace, freight movements form a large part of the transport sector. The extent of the impact will depend on whether (or not) the Withdrawal Agreement and the Protocol on Ireland and Northern Ireland are fully implemented at the end of the current transition period, the 31st December 2020.

Significant investment in Ireland's ports will play a major role in safeguarding and enhancing Ireland's international connectivity which is fundamental to Ireland's international competitiveness, trading performance in both goods and services and enhancing its attractiveness to foreign direct investment. The importance of this objective cannot be understated in the context of the UK's exit from the EU.

As the economy grows the ability of our ports to respond by adding capacity and adjusting to new environmental and technological demands is imperative to ensure the sustainability of our economic success. The capacity of ports to facilitate direct shipping routes from Ireland to Europe and beyond is critical to ensure supply chains are maintained and to counteract rising costs of raw materials.

3.0 Greenore Port

Trade at Greenore Port has continued to grow steadily since DSG took ownership in 2014. Growth figures are outlined in the Table below and highlights that the throughput has more than doubled in the last 6 years.

	2014	2015	2016	2017	2018	2019	2020
Total Tonnage	502,304	620,673	630,498	789,150	905,000	1,024,820	1,105,000
Dry Bulk (agri feed, fertiliser, rock, salt)	334,465	425,861	433,531	631,276	737,000	825,388	921,000
Break Bulk	167,839	194,812	196,967	157,874	168,000	199,431	124,000

BREAKDOWN OF TONNAGES 2014 - 2020

Greenore Port comprises a landbank of approx. 23. hectares. There are 2 no. active berths (Berth 1 and 2) and a potential 3rd berth. Improvement works to Berth 2 are underway and the anticipated completion date is Q3 2021. There are existing and planned storage warehouses and open storage areas within the landholding.

Our Client recently acquired a site, measuring c.0.42 hectares with frontage onto the Shore Road adjoining their existing landholding. The site consists of a single-storey building which is currently vacant and is screened along its western and southern boundaries by mature hedgerows and trees. It is envisaged that this area will subject to securing planning consent act as an extension to the existing open storage area.



RECENTLY ACQUIRED LANDHOLDING

3.1 Vision

It is a requirement under Policy Objective TC41 of the Louth County Development Plan 2015-2021 that a masterplan is prepared for each of the ports (Drogheda, Greenore, Dundalk and Clogherhead) in County Louth:

Policy TC41: To support the development and expansion of the ports of Drogheda, Greenore, Dundalk and Clogherhead subject to the preparation of a masterplan and compliance with all relevant EU policies such as Water Framework, Habitats, SEA and EIA Directives.

Noting the objective for the preparation of a Masterplan, a **Vision Document** has been prepared, setting out the strategic approach to the future development of lands under the control of the Applicant and provides an indicative spatial arrangement of envisaged future development proposals required to support the port. The document was prepared with a view to providing all relevant port stakeholders with an overview of the port's future plans for development. It is included with this submission under separate cover.

This document has been presented to senior officials of Louth County Council and was submitted with the two most recent planning applications made for extensions and modifications to, and the proposal for new warehousing at the Port (Reg. Ref. 20/268 and 20/543) and is available publicly for viewing on the Council's online planning portal. In addition, the Vision Document was presented to a number of Greenore residents and Tidy Towns committee members at an evening hosted by Greenore Port Unlimited in their offices at the Port in 2019.

3.2 Heritage

The port is located adjacent to Greenore Village, which was historically constructed to provide homes for dock and railway workers. The village lies to the south west of the application area and is centre on two parallel streets; Euston Street and Anglesey Street. Euston Street comprises a terrace of two-storey dwellings on its north-eastern side and a national school, shop, and more terraced dwellings on the opposite side.

Greenore Village is designated as an Architectural Conservation Area (ACA) in the extant Development Plan. The ACA starts at the bungalows on the southern outskirts, and includes Euston Street, Anglesey Terrace, and the coastguard houses. The lands under DSG ownership are not located within the ACA.

There are 3. protected structures located within the wider Greenore Port landholding including

- i. a watertower (Ref. LHS009-001),
- ii. the Greenore Lighthouse (Ref. LHS009-001), and
- iii. the lighthouse keeper's house (Ref. LHS009-044).

Lack of investment in conserving the port's infrastructure in the past has left a costly legacy for our Client in terms of overdue repairs. DSG have commissioned the preparation of a **Heritage Strategy** (included under separate cover) to guide the safeguarding of the port's built heritage in the medium term (5 – 10 years). This document should be read in conjunction with the Greenore Vision Document.

Commercial operation pays for the expenditure incurred in the upkeep of the heritage assets. A programme of rehabilitation works are currently underway on the Lighthouse and due for completion shortly.

DSG intend to submit an application for funding under the 2021 Built Heritage Investment Scheme to assist with the further repair and conservation of protected structures within the Port and are seeking Louth County Council's support in this process.

3.2 Employment

The Port is a significant employer in Greenore and the wider region of the Cooley Peninsula, with over 90% of the Port's employees living within 12km of the Port. Under previous management Greenore Port had suffered a prolonged period of sustained under investment, and the port's immediate priority was to undertake works to improve neglected infrastructure.

DSG are currently in the process advancing a programme of much needed improvement works at the Port, a major part of which includes the expansion of existing warehousing facilities to respond to the existing and significant shortfalls in storage capacity. In recent years, the increase in goods handled at the port cannot be met by the existing storage capacity, which has necessitated the movement of goods to 3rd party storage facilities prior to onward movement.

In order to acquire planning permissions to facilitate the necessary expansion to address this shortfall, DSG has been engaging with Louth County Council and has submitted several planning applications.

4.0 Draft Louth County Development Plan 2021-2027

4.1 Settlement Hierarchy

It is noted that Greenore is designated as a Level 5 settlement, Rural Village within the draft Development Plan. These are areas characterised by residential development with limited services and facilities with an emphasis on strengthening their fabric and creating sustainable rural communities, suitable to meet rural generated housing needs. No new land is zoned in Level 5 settlements for residential development.

4.2 Zoning

Unlike the extant Development Plan there is no proposed land use zoning map for Greenore in the draft Development Plan and this is consistent with all Level 5 settlements. Instead, the draft Plan defines a development boundary and an Architectural Conservation Area for Greenore.

We wish to highlight that Greenore Port is unique and distinct from other Level 5 settlements, in its industrial character and important function as the County's second largest port.

The extant Plan includes Map 2.23 contained within Appendix 2 that establishes different land use zoning designations within Greenore. This is beneficial in that it brings certainty for our Client and establishes for all stakeholders the limits of acceptable development.

It is respectfully submitted that given the strategic nature of Greenore Port that the forthcoming Development Plan should include a land use zoning map that facilitates the future development of the port. Accordingly, it is requested that the entirety of the lands under the control of our Client be zoned 'port/port related activity', consistent with the extant Development Plan.

In the absence of a land use zoning objective, the port lands are un-zoned lands. This may have unintended consequences for land use regulation and in the context of the future growth strategy for the port, this approach is not desirable. Having certainty for all stakeholders will assist with ensuring the continued success of this strategic asset.

Recommendation: Include a zoning objective 'Port/Port related Activity' on Map 5.10 of the draft Development Plan for all lands associated with Greenore Port.

4.3 Port Related Policies

Section 5.10 *Marine Opportunities Including Ports* acknowledges Greenore Port's standing as a Port of Regional Significance, as set out in the National Ports Policy 2013.

Our Client welcomes the support that the draft Plan provides for the future development of ports in County Louth, specifically, the assertion to '*support the expansion and improvements to Port facilities in the County in order to maintain connectivity and competitiveness and support the economic growth of the County and wider region.*'

It is noted that the draft Plan includes **Policy Objective EE26** that seeks to support any improvements or expansion to port facilities subject to the preparation of a Masterplan and appropriate environmental

considerations. This policy mirrors an existing policy in the current Development Plan. There is no defined timeline for a Masterplan. However, it is noted that other port masterplans generally encompass a 20-40 year timeframe.

These ports are State owned and have access to EU grants including the TEN-T Programme (Multi-Annual Work Programme and Annual Work Programme), Cohesion funds, ERDF and the Research Framework Programme. 31.7 billion euros is allocated to transport under the Connecting Europe Facility of the MFF (Multi-Annual Financial Framework) which effectively acts as "seed capital".

Being privately owned, Greenore Port is unique amongst Irish ports, and it does not have access to this EU grant aid. Essentially, its funding is sourced privately and this limits our Client's ability to provide a Masterplan of the same or similar duration as other State owned ports. Accordingly, a Vision Document has been prepared and it outlines a realistic spatial planning framework for the Port's development over the coming 5 years.

The issue of a Masterplan is regularly cited in observations on planning files associated with individual development proposals at Greenore Port.

Recommendation: An opportunity now exists to bring clarity to this matter and it is respectfully submitted that the wording of EE26 be amended as follows;

To recognise that the Port facilities at Drogheda, Greenore, Dundalk, and Clogherhead are an important economic resource and to support any improvements or expansion to these Port facilities at Drogheda, Greenore and Clogherhead and the consolidation of Dundalk Port, subject to the preparation of a Master Plan with the timeframe of same to be agreed with the Planning Authority and appropriate environmental considerations.

Figure 5.5 contained in Section 10 of the Draft Plan sets out the tonnages handled by Louth Ports in 2019 and highlights the volume of goods passing through the ports, and is an indication of the importance of these facilities in providing international connectivity to business and industry in the region. Drogheda, Greenore, and Dundalk Port account for 58%, 39% and 3% respectively of goods handled by Louth ports in 2019.

Drogheda Port, is acknowledged in the Draft Plan as one of the leading ports in Ireland, with a significant regional and economic importance in the handling of goods. As is appropriate, the Draft Plan has provided for distinct policy objectives (SS9 & EE34) which seek to support the expansion of Drogheda Port specifically, and acknowledge its economic importance.

Dundalk Port accounts for only 3% of goods handled in 2019 across the three main commercial ports in Louth. The Draft Plan has provided for a specific policy objective (EE43) for the support and growth of its development.

Over the period 2015-2019, the throughput at Drogheda Port has increased by approx. 77% and Greenore Port has increased by approx. 40%. This highlights the significance that both ports have within the County. However, the Draft Plan fails to provide any policy objective specific to Greenore Port, similar to those included for Drogheda and Dundalk ports. This absence of a specific policy in the draft Plan undermines the importance of Greenore Port.

Recommendation: In order for all ports in the County to deliver on national and regional planning objectives, it is essential that the Development Plan recognises the role of each port. Accordingly, it is submitted that a specific policy similar to SS9 and EE34 be included for Greenore Port.

4.4 Heritage Policies

Greenore Port sits 'cheek by jowl' with the village, a designated Architectural Conservation Area. The village was borne from the development of the port.

The close proximity of residential and an industrial land use must be carefully managed and each should be given equal weighting.

An efficient port requires heavy infrastructure in the form of large plant, gantries, cranes etc. and structures to store goods prior to onward distribution.

The draft Plan includes policy objectives to protect ACAs and views. It is necessary when formulating policy to establish the local context. For example Appendix 11 of the draft Plan establishes local views that require protection including outward from Greenore village along Euston Street, north to the Mourne Mountains. The draft plan fails to acknowledge that the port sits at the northern end of Euston Street and in the foreground of the Mourne Mountains. The protection of heritage and views should be considered in the round and using performance based criteria. Policy should not be an either-or proposition.

Any development within the port's harbour area would be on the periphery of the ACA. Policy Objective BHC 26 and 30 and objectives contained in Appendix 11 requiring height and scale of new development to respect the character of historic architecture do not align with the needs of a commercial port.

Our Client has no difficulty with ensuring that future development is carried out in a manner sympathetic to the special character of the village. However, a balance between the need for change and the objective of retaining the special qualities for which the area was designated must be integrated into the objectives for the ACA.

The forthcoming Development Plan should acknowledge the village's connection with the port and heritage objectives must be balanced with allowing the port fulfil its trading role in the future.

Recommendation: It is important that the forthcoming Development Plan recognises the need for balance between these conflicting objectives and, if possible, that it identifies principles as to how such conflicts can be reconciled.

5.0 Recommendations and Conclusions

Our Client operates as a privately-owned, commercial port-operator with a proven track record as an important provider of port services and the second-largest handler of port goods in County Louth. As a commercial entity, DSG has both an obligation and commitment to meeting the needs of its customers.

Staying connective and competitive in a globalised society is critical to the State's economic success. Ireland's connectivity as an island nation on the periphery of Europe and the uncertainty of Brexit are key challenges. Comprehensive policy and actions are thus critical to facilitating the development of robust port facilities and supporting businesses in getting goods on and off the island.

Currently, one of the main challenges facing Greenore Port is the shortfall in available storage capacity.. For Greenore Port to grow and remain competitive, it is critical that its future development is directly supported by the relevant national, regional, and local planning policy.

The recent publication of the Draft Louth County Development Plan 2021-2027 is welcomed by our Client, particularly the inclusion of policy objective EE26 which seeks to recognise the economic importance of the four operational Ports in County Louth (Drogheda, Greenore, Dundalk and Clogherhead).

However, despite our Client's support for the overarching policies and objectives set out in the Draft Plan in relation to ports in County Louth, a number of matters of concern has been raised in this submission. We therefore wish to make the following comments and recommendations for consideration of Louth County Council in their preparation of the Draft Plan:

- i. Under the Draft Plan, Greenore has been relocated within the Settlement Hierarchy as a '*Level 5 Rural Settlement*', for which no land-use zoning maps are being prepared in the forthcoming Plan. First and foremost, it should be noted in the Draft Plan that Greenore is distinct from other Level 5 Settlements in the context of its industrial heritage and important function as Ireland's only privately owned port and does not function solely as a small residential settlement. Accordingly, it

- is submitted that a land use zoning map be prepared for Greenore and all lands in port ownership should be zoned 'Port/Port related Activity'
- ii. Should the planning authority consider that it is not appropriate to prepare a land use zoning plan for Greenore, policies should be strengthened to ensure that the growth and development of the port estate is not undermined by this omission.
 - iii. Clarity with respect to the issue of master plans for ports is sought specifically as it relates to timeframes.
 - iv. The inclusion of an overarching policy objective (PO EE26) supporting the development the four main ports in the county and acknowledging their regional and economic importance is welcomed. However, the Draft Plan, despite its inclusion of specific policy objectives supporting the growth and development of both the Drogheda and Dundalk ports, has not provided same for Greenore Port. It is respectfully submitted that the absence of a specific policy objective for the port fails to recognise its significant and important contribution to the provision of port services in Ireland. This coupled with the absence of a land use plan could lead to adverse unintended consequences.
 - v. Greenore Port wish to work with Louth County Council to secure the future of Protected Structures within the port's estate and in this regard are seeking support with respect to securing grant aid to assist with implementing the port's heritage strategy.
 - vi. The village of Greenore exists because of the port. Policies with respect to heritage protection must seek to achieve a balance between protecting the heritage value and accommodating a commercial port.

On behalf of our Client, Doyle Shipping Group, we respectfully request that relevant draft planning policy objectives are reviewed and amended as necessary having regard to the information included in this submission..

Please do not hesitate me if you wish to discuss any aspect of this submission.

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D07 K271

November 2019

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1. Introduction

John Cronin & Associates have been instructed by **Greenore Port UnLtd** to deliver a *Built Heritage Strategy* for Greenore Port. The *Built Heritage Strategy* is intended to provide guidance for the protection and maintenance of cultural heritage assets within the port's landholding in harmony with the continued working of the port. This *Built Heritage Strategy* will be used as the basis to manage the features of the historic built environment within the port during the development of the port facilities over the next five to ten years.

It is intended that this document should be reviewed and added on a five-year cycle or as necessary. This may include a condition survey of features of architectural merit and review of plans for further development.

The *Built Heritage Strategy* for Greenore Port has been prepared with due regard to the principal policies set out in the *Greenore Vision Document 2018*.

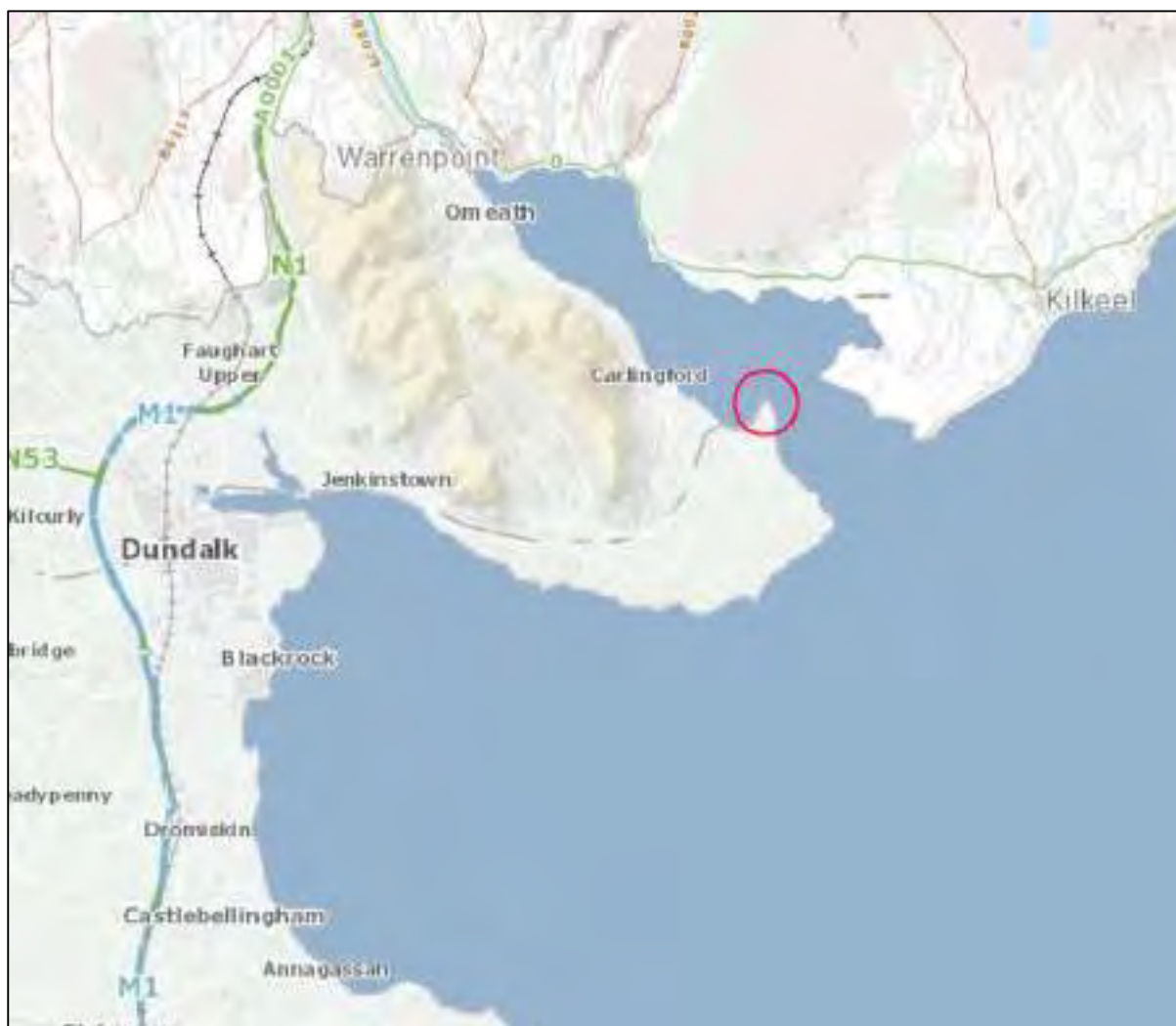


Figure 1: General location of Greenore Port (red circle) (Source: <https://www.heritagemaps.ie>)

2. Aims & Objectives

The Aims of the *Greenore Port Built Heritage Strategy* are most simply expressed as the following:

- To develop and maintain the port without impacting on sites of recorded architectural heritage.
- To protect and enhance the features of the historic built environment within the port.
- To seek a marriage of preservation/conservation of features of architectural/historical value with the development and working of the port both at present and into the future.

In light of the above-mentioned aims, the objectives may be summarised as follows:

- To identify the principal issues and establish a strategy to address these.
- To manage the challenges of maintaining historic built environment in the presence of a busy port environment.
- As custodians of the site, Greenore Port UnLtd. to take a proactive role in caring for the local heritage within their site.

3. Context

Location

Greenore Port is located on Greenore Point on the southern coast of Carlingford Lough, approximately 3.3km east of Carlingford and 15km northeast of Dundalk, County Louth. The townland of Greenore is within the Civil Parish of Carlingford. Greenore Point projects northwards forming, along with Greencastle Point, County Down a pinch point in Carlingford Lough, while also providing a sheltered deep-water natural harbour on its western shore.



Figure 2: Location of Greenore Port (Source: <https://www.heritagemaps.ie>)

Legal & Policy Framework

The management and protection of cultural heritage in Ireland is achieved through a framework of national laws and policies which are in accordance with the provisions of the Valetta Treaty (1995) (formally the *European Convention on the Protection of the Archaeological Heritage*, 1992) ratified by Ireland in 1997; the *European Convention on the Protection of Architectural Heritage* (Granada Convention, 1985), ratified by Ireland in 1997; and the *UNESCO Convention for the Safeguarding of the Intangible Cultural Heritage*, 2003, ratified by Ireland in 2015.

Architectural Heritage

Protection of architectural heritage is provided for through a range of legal instruments that include the Heritage Act (1995), the Architectural Heritage (National Inventory) & National Monuments (Misc. Provisions) Act (1999), and the Planning and Development Act (2000).

The Heritage Act (1995) (as amended) defines architectural heritage as including: *all structures, buildings, traditional and designed, and groups of buildings including streetscapes and urban vistas, which are of historical, archaeological, artistic, engineering, scientific, social or technical interest, together with their setting, attendant grounds, fixtures, fittings and contents.*

The National Inventory of Architectural Heritage (NIAH) was established under the Architectural Heritage Act (1999), to record architectural heritage structures within the State and to advise local authorities in relation to structures of architectural heritage significance within their administrative areas. The conservation principles of care and protection of architectural heritage and the facilitation of the listing of significant buildings of architectural merit are set out in Part IV of the Planning and Development Act (2000). This requires Local Authorities to maintain a Record of Protected Structures (RPS) of structures with special architectural, historical, archaeological, artistic, cultural, scientific, social or technical interest, to be included in City/County Development Plans. In addition, Local Authorities must provide for the preservation of townscapes etc. through designation of Architectural Conservation Areas (ACAs). Any changes that materially affect the character of a protected structure require planning permission.

The *Louth County Development Plan 2015-2021* presents a number of objectives to ensure the protection of the architectural heritage resource within the County and these include to: “Protect and enhance Louth’s Heritage (built and natural)”

Louth County Development Plan 2015-2021 states that:

The architectural heritage comprises visible buildings and other structures/ which are located above ground and were broadly (but not exclusively) constructed since 1700AD. The local authority has primary responsibility, under the Planning Acts, for the conservation of built heritage. The protection and management of this built heritage is achieved through the designation of ‘Protected Structures’ (inclusive of their attendant grounds, including designed landscapes, parks and gardens) and of Architectural Conservation Areas (ACAs). Buildings of architectural importance can range from domestic houses of local importance to stately homes and buildings of national or international importance.

The majority of the nineteenth-century structures at Greenore are designated protected structures in the current *County Louth Development Plan (2015-2021)* and listed on the National Inventory of Architectural Heritage (NIAH), this includes the lighthouse, lighthouse keeper’s house, the coastguard house and the majority of terraced housing within the village.

Historical background

Carlingford Lough comes to a pinch point between Greenore (County Louth) and Greencastle (County Down). Not only does this natural formation constrain water flow creating strong tidal currents, but the opposing points of land extend out to each other and act as natural points in which to control access into and out of the Lough. These points would have provided the location for important navigational aids for boats and ships. Indeed, although there is no settlement in Greenore, Greenore Point is clearly depicted and annotated on the Down Survey map (circa 1655-

1658), showing its importance for the navigation of Carlingford Lough (<http://downsurvey.tcd.ie/down-survey-maps.php>). This natural topography influenced the siting of the lighthouse, lightkeeper's house and coastguard station - indicated on the first-edition Ordnance Survey map (see **Figure 3** below) - erected in the early nineteenth century. Lewis's Topographical Dictionary of Ireland, Vol. 1 records a small lighthouse with a revolving light and a coast-guard station on Greenore Point (Lewis 1837, 254).

The modern village and settlement at Greenore is largely due to a nineteenth-century commercial venture by the London & North-Western Railway Company (LNWR) who identified the area as a suitable location for a cross-channel ferry service to Holyhead (Wales). However, it was the natural situation of Greenore that influenced the location of this planned Victorian village.



Figure 3: Extract from first edition O.S. map (c.1835) depicting the Greenore area prior to the port and commercial railway developments of the 1860's and 70's

Works began in 1863 and plans included a deep-water port facility with railway connection (passenger terminus, a goods shed, cattle pens, signal box and posts etc.). The route was also envisaged as being attractive to the wealthier classes and in this respect a hotel was built, together with accommodation and facilities for personnel (terraced housing (Euston Street), gasworks, reservoir, shop and school). The port of Greenore was opened in 1867 and a ferry service operated to the port towns of Heysham and Fleetwood in Lancashire, England.

The Newry & Greenore Railway opened on 1st May 1873 and was constructed to connect the town of Newry with the LNWR port at Greenore. From 1873 a ferry service between Greenore and Holyhead in North Wales was introduced. The name of the railway was changed to the Dundalk,

Newry & Greenore Railway in 1873 and a new line which connected to Dundalk was opened in 1876. In total the track was 26 miles in length and operated on a 5 foot 3-inch gauge. It became part of the London, Midland and Scottish Railway (LMS) in 1923, and under an agreement of 1933 it was operated by the Great Northern Railway (Ireland) (GNRI) on behalf of the LMS. The railway line, however, closed in on 31st December 1951 after the withdrawal of the ferry services from the port of Greenore.



Figure 4: Contemporary map depicting the route of the Dundalk, Greenore & Newry Railway as well as a portion of the Great Northern Railway (Ireland)



Figure 5: Extract of photograph of Greenore harbour taken by Robert French (1880-1900)
 (Source: The Lawrence Collection in the National Library of Ireland)

Cartographic review

The first edition Ordnance Survey (OS) map depicting Greenore (see **Figure 3** above) records the lighthouse complex which survives today. At this time, the lighthouse lay at the very north end of Greenore Point, just slightly removed from the High-Water Mark (HWM) and representing the tip of the natural foreshore. No features are indicated on the shoreline on this map edition, which is shown as an uninterrupted expanse of shingle.

The 25-inch O.S. map (**Figure 6**) is from shows that land to the north of the lighthouse has been reclaimed. The west-facing shoreline of Greenore Point was infilled to accommodate the construction of the railway station and associated harbour. These works included the extension of shoreline protection measures around Greenore Point. The area was subsequently reclaimed further and much of this earlier detail was covered over by the growth of Greenore Port. Further reclamation is evident extending seaward to absorb the former high-water mark in front of the lighthouse. The area is now a car park and accommodates a newly built car ferry slipway.

The 25-inch O.S. map also show the hotel and station that was built in the 1870s to designs by James Barton (Barton was a pupil of the noted engineer John MacNeill). Though now demolished (save for a section of walling of the former station), this complex was once an integral part of harbour and railway life. Amenity resources were added to Greenore during the decades after the opening of the railway line; these included a golf course, holiday bungalows, tennis courts and a croquet lawn. The Greenore rail and cross-channel ferry services ceased in 1951 and the railway

track itself was dismantled in September 1954. Although the railway line closed the commercial port continues to operate.



Figure 6: Extract from 25-inch edition O.S. map (produced between 1888-1913) depicts Greenore after the development of the port, railway and ancillary infrastructure

4. Historic built environment of Greenore Port

There is a small number of features within the port area that are of architectural interest. Of the four buildings recorded in the National Inventory of Architectural Heritage (NIAH) three are listed on the Record of Protected Structures (RPS) – the exception being the hotel and the former railway station of which little survives (see **Table 1** below). In order for a structure to be listed by a planning authority on the RPS it would have to be a building or part of a building that is considered to be of special interest from an architectural, historical, archaeological, artistic, cultural, scientific, social, or technical point of view. This means that the RPS structures within the port area are of unquestioned quality and importance to the overall historic built environment of Greenore – being intrinsically linked to the establishment and development of the village - and County Louth.

Table 1: Structures listed in the NIAH within Greenore Port

NIAH Reg.	Identification	Designation	Description
13831026	Greenore Hotel and Railway Station	NIAH	Detached fourteen-bay two-storey with attic brick former railway station and hotel, built c. 1875, attic level later addition. Now disused. Single-bay two-storey fully-engaged block to south-east with canted bay window and octagonal timber-framed gazebo, double-pile return to north, flat-roofed stone porch and single-storey flat-roofed block to south, attached eight-bay single-storey with attic block to west. Double-pile mansard slate roof, clay ridge and hip tiles; stepped red brick parapet on limestone cornice, cut limestone coping; red brick shouldered corbelled chimneystacks, cut limestone dressings and caps; paired flat-roofed dormer windows, carved timber cornices, flanked by fluted pilasters; limestone verge coping to west block; parapet gutters, iron hopper-heads, square-profile cast-iron downpipes. Red brick English garden wall walling, cut stone plinth coping and string courses, dressed ashlar banding, block-and-start dressed granite quoins, continuous stone lintel course to south block, brick dog-tooth course to north elevation. Square-headed window openings, moulded stone surrounds with keystones, limestone sills on stone corbels, painted timber one-over-one and two-over-two sliding sash windows; ashlar sandstone surrounds to canted bay windows, stone mullions and transoms, timber fixed frame windows; Wyatt window to first floor south-east block, sandstone lintel, keystones, one-over-one sliding sash windows; tripartite windows to north elevation, block-and-start bull-nosed yellow brick jambs, timber lintels, one-over-one sliding sash windows; segmental-headed window openings to west, rubbed

NIAH Reg.	Identification	Designation	Description
			brick lintels. Ashlar limestone porch, moulded cornice, square piers with round-headed panels flanking round-arched openings to south and west elevations, stop-chamfered corners, cut stone impost mouldings voussoirs and keystones, cut limestone steps; round-headed door opening, smooth rendered surround with keystone, painted timber double doors with glazed panels and sidelights; square-headed door opening to east end south elevation, moulded stone surround, keystone, painted timber four-panel door, c. 1900, plain-glazed overlight; square-headed door opening to north, brick surrounds, timber doors. Located to south of quay with harbour-related structures to south and Greenore Point to north-east.
13831044	Greenore Lighthouse Keeper's House	RPS: LHS009-044	Detached three-bay single-storey with attic former lighthouse keeper's house, built c. 1830, now disused. Rectangular-plan, porch with lean-to roof projecting from north and south elevations, lean-to flanking bay to east. Double-pile pitched slate roof, clay ridge tiles, granite verge coping, granite chimneystacks with stone strings and red brick top stages, cast-iron gutters on drive-in brackets, circular cast-iron downpipes. Painted smooth rendered walling, painted stone plinth. Square-headed window openings, stone sills, painted timber two-over-two, six-over-six and eight-over-eight sliding sash windows. Square-headed door openings, painted timber vertically-sheeted doors, cast-iron door furniture. Set in grounds shared with lighthouse to south-west, bounded by random rubble stone wall, painted to east, granite coping, square gate piers with pyramidal caps to east; located to south of Greenore Point.
13831043	Greenore Lighthouse	RPS: LHS009-043	Freestanding two-stage lighthouse, built 1830, now disused. Circular-plan, tapered profile. Flat roof with domed light positioned centrally on top, surrounded by walkway with wrought-iron railing, weather vane to light. Painted ashlar stone walling, projecting plinth, corbelled brackets supporting roof. Square-headed window openings, painted stone sills, painted timber eight-over-eight sliding sash windows. Square-headed door opening, painted timber vertically-sheeted door. Interior with granite and concrete floor, smooth rendered walling, circular granite staircase, cast-iron balustrading, fluted columns, original lamp missing. Set in grounds

<i>NIAH Reg.</i>	<i>Identification</i>	<i>Designation</i>	<i>Description</i>
			shared with lighthouse keeper's house to north-east, bounded by random rubble stone wall, painted to east, granite coping, square gate piers with pyramidal caps to east; located to south of Greenore Point.
13831025	Water tower	RPS: LHS009-001	Attached three-stage stone water tower, built c. 1840, ground floor converted to office lobby with office buildings attached to east and west. Square-plan, steel tank on yellow brick corbel course. Squared roughly coursed rubble stone walling to bottom stage, red brick walling to upper stages; cut limestone plinth coping, yellow brick quoins, ashlar limestone and yellow brick string courses. Blind oculus to bottom stage, yellow brick surround; blind paired round-headed openings to second stage, block-and-start yellow brick jambs, round-arched brick archivolt. Round-headed door opening, block-and-start yellow brick jambs, round-headed rubbed brick archivolt, painted timber door with glazed panels. Two-storey random rubble store to north c. 1840.

Critical appraisal of structures

Built heritage assets and their settings are vulnerable to change, including natural processes such as erosion, decay and neglect, as well as from development. Each of the four built heritage assets as well as having a group value have an intrinsic value specific to that structure. The following section will involve an evaluation of the current condition of each of the structures and assess their individual vulnerabilities.

Greenore Lighthouse

The circular-planned lighthouse which retains a tapered profile and the Lighthouse Keeper's house, were both built c. 1830 and are located within a discrete courtyard, as indicated on the first edition Ordnance Survey map. The lighthouse was built by George Halpin Senior and is regarded as an important example (see **Figure 7 & Plates 1 - 8** below). This is the earliest building on the site and is part of the reason that the port was sited at this location, which in turn is the reason that Greenore Village was constructed.

The stone structure is sound and the openings have been blocked up and the exterior whitewashed. The light structure consists of a glazed dome of copper-alloy construction. The door to the lighthouse dome is open and cannot be shut properly, which is allowing water ingress, which will over time cause damage to the structure. This door must be repaired as a matter of urgency.



Figure 7: Extract of photograph of Greenore Lighthouse and Keeper's House taken by Robert French (1880-1900) (Source: The Lawrence Collection in the National Library of Ireland)

Greenore Lighthouse Keeper's House, boundary wall & outbuilding

The Keeper's House consists of a detached three-bay single-storey structure, built c.1830 to house the lighthouse keeper and his family (see **Plates 12 - 14** below). Located at the northeastern end of the port close to the main heavy vehicle access and ferry terminal, this site (including the lighthouse and outbuilding) is delimited by a polygonal boundary wall.

As with the lighthouse, the keeper's house has a sound structure and the openings have been boarded up to prevent water ingress. The roof structure requires considerable repair and augmentation in terms of the internal timber elements. The internal ceilings are water damaged and it appears that rainwater entering the structure through gaps around the chimneys and potentially through the roof valley. Vegetation growth in the valley may be hampering rainwater dispersal into downpipes. If a critical mass of rainwater builds up in the valley, this has the potential to cause considerable damage. A simple and straightforward part-solution would be for lead flashing and rainwater goods to be checked, repaired and maintained

The site boundary consists of whitewashed random rubble wall with cut stone coping (see **Plates 9 - 11** below). A concrete ladder cover (slightly damaged) is attached to the interior of the southwest wall elevation. Several longitudinal cracks are visible – some running for considerable lengths – particularly to the southwest and northwest walls. Sections of these walls should be taken down and rebuilt with authentic and sympathetic materials (re-use existing stone). A crash barrier or timber baulk should be erected surrounding the exterior of the wall to prevent further inadvertent damage by port vehicles.

The single-storey lean-to outbuilding likely dates to the same period as the lighthouse and keeper's cottage and although not a Protected Structure in itself, it is both within the curtilage of the Protected Structures and an integral part of the usage of the former lighthouse. The walls are of whitewashed random rubble construction; the northwest and northeast elevations are formed by the corner of the boundary wall. It is roofed in the same manner as the keeper's house with natural slate and cut stone coping.

The timber rafters and joists have rotted as a result of moisture ingress. The internal ceilings have collapsed and there is overall subsidence in the roof structure. The openings include broken windows, which assist with water ingress. Urgent works required to the roof structure and openings.

Greenore Hotel and Railway Station

Greenore Hotel and Railway Station was an impressive edifice, built c.1875 and which underwent extension and transformation over a one-hundred period (see **Figures 8 - 10**). Being linked to the railway which ceased operating in 1951, the hotel closed soon after. The DN&GR was dissolved in 1957 and the port came into private Ownership (Kharchenko 2012, 647).



Figure 8: Extract of photograph of Greenore hotel taken by Robert French (1880-1900)
(Source: The Lawrence Collection in the National Library of Ireland)

As evidenced from the photographs of the Lawrence Collection (**Figures 8 & 9**), the Hotel underwent extensive alterations in the early twentieth century, including the addition of a third floor (dormer), alterations to ground floor window openings and the addition of a ground floor

covered veranda, etc. Thus, the building surveyed by the NIAH was the early twentieth century iteration of the Greenore Railway Hotel.

The hotel and most of the railway station was demolished c.2006. Fragmentary remains of part of the railway station are extant within the port, consisting of a redbrick arcade (see further below).



Figure 9: Extract of photograph of Greenore hotel taken by Robert French (1865-1915)
(Source: The Lawrence Collection in the National Library of Ireland)



Figure 10: Photograph of Greenore hotel taken by James Green (circa 1990)
(Source: The National Library of Ireland)

Free-standing, eleven-bay (six arches extant) blind arcade wall, formerly associated with Greenore Railway Station, built c.1870-1890. Orientated northeast to southwest, with main façade facing northwest, measuring 61m in length and approximately 8m in height. Perforated redbrick to northwest elevation, dressed limestone blocks built to courses to southeast (rear/external) elevation. Ashlar limestone coping with ashlar limestone block-and-start quoins and cornice to southwest end. Ashlar limestone tie-in bracket (paired to southeast elevation) to spandrel. Northwest elevation comprising round-headed arcading of block-and-start bull-nosed yellow brick; yellow brick string course to springline of arches. Northeastern portion of wall (comprising six arches) lowered to form low wall with modern Portland cement rendered coping (see **Plates 17 - 22** below).

This arcade represents the extant remnants of the southeast elevation of the former railway building on the site. This now free-standing structure was not considered worthy of recording as part of the NIAH record for the hotel & railway station (NIAH ref. no. 13831026). Being fragmentary and having lost its context and the gravity of association with the hotel building (demolished c.2006), this feature no longer retains its intrinsic architectural value nor does it contribute positively to the character of the port. Furthermore, it has no designation under any relevant heritage policies and is not subject to any orders for preservation. Moreover, this freestanding feature has become an impediment to port activities and has the potential to become dangerous. However, given its former value in association with a former railway station structure and taking into account the fact that built heritage is a finite resource, it is recommended that this wall is subject to full industrial archaeological survey prior to removal (see further below).



Figure 11: View of northwest elevation of redbrick wall, facing northeast

Water tower

The water tower is an integral part of the historic environment of the port. It was built to service the railway station, c.1875. Although the function of this structure has changed (it currently functions as office space for port administration), this has meant that the structure has been preserved and maintained.



Figure 12: Extract of photograph of Euston Street, Greenore with water tower in background (1900-1939) (Source: Eason Photographic Collection in the National Library of Ireland)



Figure 13: View along Euston Street facing towards the port

4. Critical issues

The underlining port development strategy that this *Built Heritage Strategy* takes account of is one of sustained expansion of capacity and extension of facilities within the port area; the *Built Heritage Strategy* seeks to ensure how the protected structures within the site (and thereby in the ownership of Greenore Port UnLtd) are preserved and maintained into the future. Maintaining a balance between development and conservation in a busy port that contains features for built heritage is a difficult prospect. However, Greenore Port UnLtd are committed to the preservation and conservation of these structures now and into the future. Greenore Port UnLtd through their agents will explore opportunities and seek sources of funding to undertake repair and conservation works to safeguard the structures. These works will be undertaken by qualified conservation specialists under the supervision of built heritage/conservation professionals.

In order to maintain the functioning and expansion of the port, it is necessary to expand the current port facilities. In doing so, some existing walls within the port area will need to be removed, including a redbrick arcade, which was associated with the former railway station on this site and a rubblestone wall (not recorded by the NIAH) near to the southwestern end of the port area. The planned construction of new warehouses will necessitate the removal of the redbrick arcade mentioned above.

Opportunities and threats

As well as assessing the vulnerabilities of heritage assets and challenges facing the built environment within the port, it is useful to present opportunities to enhance the heritage assets. Development provides opportunities for positive change, including funding for maintenance and conservation projects.

The owners of the port, Doyle Shipping Group (DSG) have invested in the region of €20 million in the port since 2015. The immediate priority was to invest in port infrastructure to make it fit for purpose. The coming years mark a turning point for the site as, DSG intend to undertake a sustained programme of development within the port. The owners are aware of the site's heritage significance and the protected nature of some of the surviving buildings on site. Unfortunately, there has been a legacy of abandonment and dereliction associated with the former lighthouse complex. Though protected under the provisions of the Local Government (Planning & Development) Acts, the complex has been vacant for many decades and no meaningful conservation works have been carried out since the lighthouse was decommissioned. Unless a programme of stabilisation and repair are progressed in the short- to medium term, the building complex (and especially the former lighthouse keeper's house) will continue to decay and deteriorate. As part of the next phase of investment in the port, DSG, intend to increase productivity in parallel with safeguarding the protected structures within the port. A thriving port would allow the landowner to invest not only in port infrastructure, it would also provide the landowner with the means to commit the significant investment that is needed to safeguard the lighthouse and keeper's cottage.

Unfortunately, the remains of the former Greenore Railway Station are fragmentary and are of little architectural interest. Given the location of the sole surviving flanking wall of the former station is within the working area of this busy port, it represents a constraint and a barrier to the smooth running of the port facility. The wall fragment is however of industrial archaeological interest and would benefit from a detailed scheme of recording (i.e. “preservation-by-record”) so that the inherent heritage significance of the remains are adequately recorded. Murphy surveys have carried out a detailed survey of this wall, including orthorectified photography of the front and rear wall elevations and architectural renderings of front and rear wall elevations from the orthorectified photography (see **Figures 14 – 17** below). If it were necessary to take this surviving wall fragment down for operational reasons, it would be imperative that a written industrial archaeological record be made, which should include reference to the detailed survey already undertaken.



Figure 14: Orthorectified view of front elevation of redbrick wall/arcade (extract from Murphy Surveys drawing package)

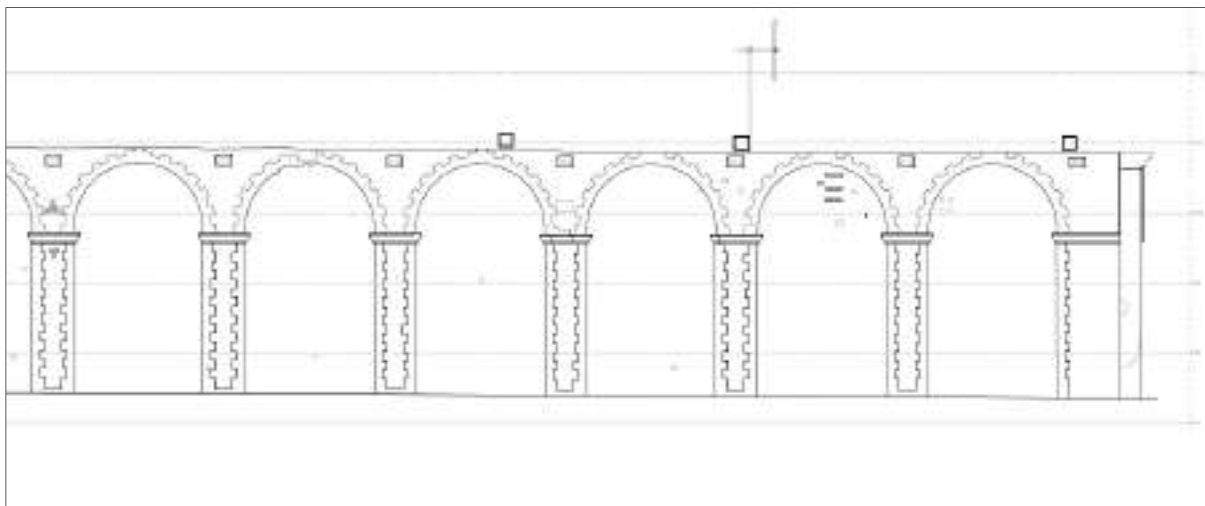


Figure 15: Architectural rendering of front elevation of redbrick wall/arcade (extract from Murphy Surveys drawing package)



Figure 16: Orthorectified view of rear elevation of redbrick wall/arcade (extract from Murphy Surveys drawing package)

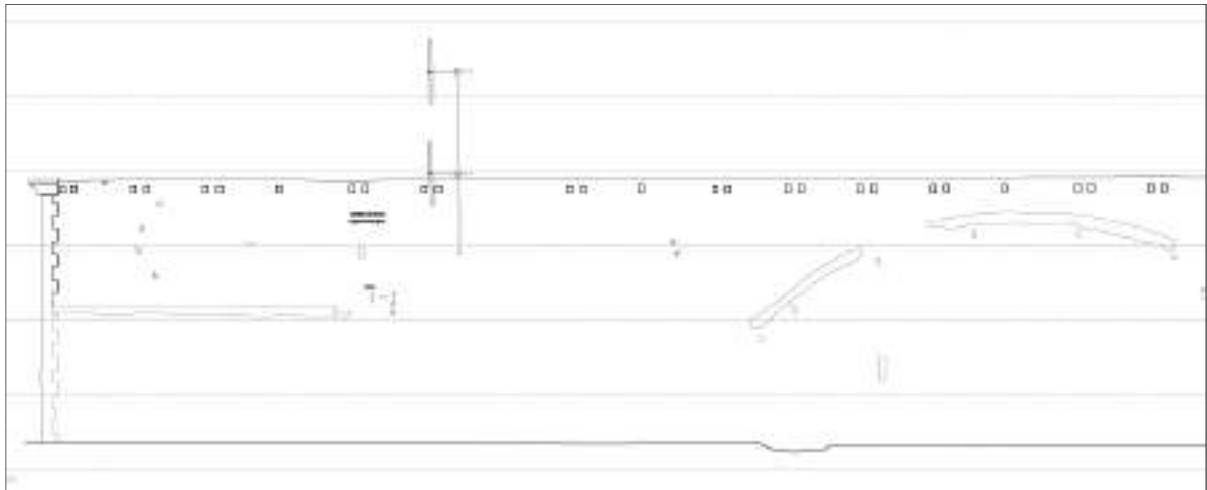


Figure 17: Architectural rendering of front elevation of redbrick wall/arcade (extract from Murphy Surveys drawing package)

5. Conservation philosophy and objectives

Conservation philosophy

The fundamental tenet of this *Built Heritage Strategy* is that key elements of the port area are of heritage significance that should benefit from long-term conservation and be managed to maximise appreciation and understanding. The policies that are set out below are based on the following underlying principles:

- The historical significance of the port warrants comprehensive, integrated conservation policies for its preservation.
- Conservation policies for the site should uphold all relevant statutory protections of its cultural heritage.
- In conserving the heritage significance of the subject lands, the landowners should expect the right to develop the property for appropriate new uses will be respected by third parties.

The *Greenore Vision Document 2018* sets out a vision for the current and future development of Greenore Port. The Conservation policies set out below are consistent with the *Greenore Vision Document*.

Conservation Policies

- **Policy 1** The landowner recognises that the special character of the site is a common good and a shared responsibility. In drafting development proposals for these lands, the landowner will be guided by the special character of the site.
- **Policy 2** The future management of the site will recognise the lawful rights of the private landowner and no measures will be undertaken that would restrict their right to enjoy their property or have access to it at any time. The landowners will develop proposals for integrating new development into the site while at the same time retaining the heritage significance of the place.
- **Policy 3** Future development within the site should be essentially sympathetic to the historic legacy of the port and the adjoining townscape of Greenore. The aim of such development would be to reutilise and re-invigorate the heritage resource, where practicable and feasible. Where direct impacts are likely to occur and are deemed unavoidable, the landowners shall endeavour to mitigate and reduce same through appropriate mitigation measures.
- **Policy 4** Any development proposals for the site should seek to respect and conserve the protected structures within the port area, namely the lighthouse and the lighthouse keeper's house. An objective of any long-term development strategy for the port should seek the conservation, refurbishment and the provision of a new appropriate re-use for the building complex. The landowners will endeavour to facilitate controlled access to the lighthouse complex once it has been made safe. In the short-term, a detailed conservation assessment and programme of repair works will be carried out by a conservation

consultant and a programme of conservation measures submitted for agreement with the Planning Authority, Louth County Council.

- **Policy 5** Greenore Port UnLtd recognise the need to appropriately document and record features or structures of built heritage fabric not formally protected by Louth County Council should there be a need to remove such features for port development.
- **Policy 6** Greenore Port UnLtd shall retain archive files of strategy documents, heritage policies, impact assessment and other documents relating to the built heritage, conservation and general history of the port, such as historic photographs, maps, plans and port files. This archive should be stored on site and made available to researchers and other interested parties.
- **Policy 7** In tandem with any proposed development of the landholding, Greenore Port UnLtd recognise the heritage, amenity and tourism significance of Greenore and the wider Carlingford Lough area. Greenore Port UnLtd will seek to be an active stakeholder in measures to promote the attractiveness of this area to visitor and resident alike. Recommended measures include:
 - Provide discreet on-site information panels/boards that would provide summary historical information and relevant historical illustrations .
 - Support the production customised leaflet/literature and ‘project packs’ for use by local schools so that the heritage significance of the port is retained and perpetuated within the local community.

Funding sources

As the owner of the site and custodian of the built heritage within it, Greenore Port UNLtd will provide funding for conservation works to ensure the long-term preservation of structures of heritage value within the port area. In addition, Greenore Port UNLtd will continue to apply for forthcoming funding schemes from local and state bodies involved in the protection of the built environment will be sought. These may include:

- *Built Heritage Investment Scheme and Historic Structures Fund* - Funding for the repair and conservation of Protected Structures and buildings within Architectural Heritage Areas (AHA) is provided by the Department of Culture, Heritage and the Gaeltacht and administered by the local authorities. Grant-aid typically requires 50% matching funds on the part of the owner/occupier and ranges between €2,500-€15,000 depending on the nature of the works. This will be open to applications for 2020 from January.
- *Historic Structures Fund* - The Department of Culture, Heritage and the Gaeltacht provides the Historic Structures Fund to replace the Structures at Risk Fund. This fund is for conservation works to heritage structures, in both private and public ownership and is administered by the local authorities.

6. Next actions

This document attempts to provide a context for the built heritage within Greenore Port and to propose a number of measures that will safeguard the built heritage in the Port's ownership into the medium term (5 – 10 years). This document can be read as a standalone report and should be read in conjunction with any planned development strategies for future works, including the *Greenore Vision Document 2018*.

It is clear that the village of Greenore would not have existed without the port having been sited at this location. The port was therefore directly responsible for the development of the village and currently is the biggest employer in the village. Urgent conservation works are required in relation to some of the protected structures within the port.

The potential impact of a previous development within the port area, namely the erection of silos was assessed by John Cronin & Associates and John Greene (Conservation Architect) in 2017. The resultant Heritage Impact Assessment predicted 'no change to setting' of the Greenore ACA. As shown in **Plate 30** below, no visual impact has occurred as a result of the construction of silos within the port.

A busy port such as Greenore Port is in a state of continual change and development. It is incumbent on a responsible landowner such as Greenore Port UnLtd to protect features the built environment within their landholding, while at the same time being able to expand and grow the port and in turn the local economy. The extant protected structures within the port (lighthouse, keeper's house & water tower) are integral to the built heritage of Greenore and indeed to County Louth. The lighthouse in particular, which is an important example of its kind in Ireland, has a landmark quality and is particularly visible to visitors using the Carlingford Ferry. This *Built Heritage Strategy* is clear in stating that developments that would impact directly on the protected structures within the port would not be acceptable. **Greenore Port UnLtd are committed to the preservation of these structures.**

The developments currently proposed within the port will not pose any direct or indirect impact on the built historic environment or on the adjacent Greenore Village ACA. However, a number of works should be undertaken to protect the protected structures within the port. The following is a short list of priority works on the protected structures:

- (1) Prevent water ingress to lighthouse and keeper's house.
- (2) Reconstruction of lean-to of store house within lighthouse complex.
- (3) Making good and re-fenestration of keeper's house.
- (4) Works to services on interior of keeper's house.

The following is an expanded list of recommendations relating to the protection of built heritage within the port:

Lighthouse complex

The valley and rainwater goods associated with the keeper's house should be cleared of obstruction and routinely maintained (inspected and cleaned each winter) to ensure normal rainwater dispersal and avoid potential water ingress from this source.

Proposals for urgent conservation works on the lighthouse, keepers house and outbuilding as presented previously should be undertaken as soon as possible (see **Appendix 2** below). These works are not inconsiderable but will be funded in part by Greenore Port UnLtd. Both the outbuilding and keepers house should be reused in order to ensure their maintenance. It is proposed that these buildings would make ideal offices and space for storage of files once conserved.

Inadvertent damage has been caused to the boundary wall to the lighthouse complex. Sections of this wall should be de-constructed and rebuilt by an experienced conservation professional. In addition, a crash barrier should be erected (approximately 0.4m) around the exterior perimeter of the wall to prevent unintentional damage. As a general rule, storage of material in the area around the lighthouse complex should be done in a structured manner to avoid untidiness.

Water tower

The water tower is currently being used for port administration offices. This change of use has greatly enhanced the structures potential to be maintained into the future. Repurposing of buildings to suit a modern commercial function – without impacting the original fabric of the building – is the most effective way of conserving and maintaining structures of historic/ architectural value. No conservation issues were noted during recent site visits; thus, no urgent conservation measures are required.

Redbrick arcade (remains of former railway station)

Proposed development at the western side of the port, which includes the extension of one warehouse and the construction of further warehouses will require the removal of a freestanding redbrick arcade associated with the former railway station (previously demolished). The demolition of the former railway station and hotel (recorded in the NIAH) has resulted in a loss of context for the redbrick arcade, which is now in a state of limbo within the port. This arcade is not a protected structure and no longer retains any cultural heritage value and furthermore is now an impediment to the development of the port.

It is proposed that this arcade is removed as part of proposed developments. However, given its erstwhile association with the former railway station, the following measures should be undertaken to preserve it by record:

- Details (measurements, individual photographs, check for makers marks/ frogs) of the cut stone and bricks should be undertaken during demolition.

Rubblestone wall

A rubblestone wall at the southwestern portion of the port area is proposed for demolition as part of port development in that area. This wall retains no intrinsic cultural heritage value and it is considered that its removal will not impact on the historic built environment. It is recommended that this wall is removed as part of proposed developments, without any additional mitigation measures.

To ensure the continued functioning of Greenore Port, further development to facilities are ineludible. There is often a perceived dichotomy between modern development and

conservation, however it is hope that this *Built Heritage Strategy* may help in some way to bridge this divide. The exigency of works required for the conservation of some built heritage features coupled with the Port's requirement for additional facilities means that a considerable monetary investment will be required. Greenore Port UnLtd are committed to the continual preservation of protected cultural heritage assets within their guardianship and have pledged funding for conservation works (see **Appendix 2** below).

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Appendix 1: Photographic record



Plate 1: View across Greenore Port facing northeast, with lighthouse in background



Plate 2: View of lighthouse and keeper's house, facing northeast



Plate 3: Aerial view of lighthouse and keeper's house, facing northeast



Plate 4: View of lighthouse and keeper's house, facing northwest



Plate 5: View of lighthouse, facing north



Plate 6: View of lighthouse, facing southwest



Plate 7: Aerial view of lighthouse, facing northwest



Plate 8: Aerial view of lighthouse showing first floor door, facing south-southwest



Plate 9: View of damaged wall surrounds of lighthouse and keeper's house, facing north



Plate 10: View of damaged wall surrounds of lighthouse and keeper's house, facing south-southwest



Plate 11: View of damaged ladder cover to wall surrounds of lighthouse and keeper's house, facing west



Plate 12: View of keeper's house and wall surrounds, facing northwest



Plate 13: View of keeper's house, facing south-southwest



Plate 14: Aerial view of valley to keeper's house, facing southwest – the internal valley is in need of cleaning and unblocking



Plate 15: Aerial view of outbuilding associated with lighthouse, showing damage to lean-to roof



Plate 16: Close-up of outbuilding associated with lighthouse, showing damage to lean-to roof, facing northwest



Plate 17: Aerial view of Greenore Port facing southeast, with redbrick wall proposed for removal in foreground



Plate 18: View of northwest elevation of redbrick wall, facing northeast



Plate 19: View of northwest elevation of redbrick wall, facing south-southwest



Plate 20: Close-up of arch to northwest elevation of redbrick wall



Plate 21: View of southeast elevation of redbrick wall, facing northwest



Plate 22: Aerial view of southeast elevation of redbrick wall, facing west-northwest



Plate 23: Aerial view of building proposed for extension, facing southeast



Plate 24: Aerial view of building proposed for extension, facing southwest



Plate 25: View of building proposed for extension, facing northeast



Plate 26: View of northwest elevation of wall proposed for removal with building proposed for change of use, facing south



Plate 27: View of northwest elevation of wall proposed for removal, facing south



Plate 28: View of southeast elevation of wall proposed for removal, facing west-northwest



Plate 29: View of Greenore Village ACA, facing south along Euston Street



Plate 30: View along Euston Street from Greenore Co-operative facing northwest. Silos within the port are not visible



Plate 31: View from no. 3 Euston Street facing northwest. Silos within the port are just visible

Appendix 2: Conservation method statement for the lighthouse complex

Introduction

Built by George Halpin Senior in 1830, the lighthouse is c.11m high and was built to highlight the southern entrance of Carlingford Lough. The lighthouse and keeper's cottage are a reminder of the maritime industry which shaped the development of Greenore. Though now disused, it retains its original form and character together with important salient features such as its lantern and other original materials.

Greenore Lighthouse, keeper's cottage, outhouse and curtilage are within the grounds of the modern Greenore Port and have fallen into disrepair. The Port wish to invest in these structures to protect them for future generations and showcase them to the growing tourist traffic using the new Greenore to Greencastle Scenic Ferry.

Supervision

All works to the external fabric of the buildings are to be carried out under the supervision of a conservation consultant (from John Cronin & Associates) and no taking down, opening up nor any feature or fitting is to be removed without his approval. The appointed contractor will be obliged to consult with and take direction from the client's conservation consultant during the course of the project. Any directions issued by representatives of the Planning Authority shall be communicated to the conservation consultant.

Experienced Contractor

Only contractors with proven experience in the repair of historic buildings are to be engaged on the work. Contractors undertaking the necessary conservation works to this protected structure should be experienced in working on such historic buildings and be familiar with the principles of best-practice conservation. This is based on the full understanding of how the buildings' fabric functions, minimal intervention required to ensure the survival of the building, maximum retention of viable historic fabric and use of replacement materials and techniques that respect the quality of the original buildings, are legible as modern interventions and are by their nature reversible without compromising the remaining historic fabric.

Protection of the property during construction or repair

During the course of construction or repair of the property its elements should be protected from damage. Precautions should also be made to protect the buildings from fire and the ingress of water.

Structure generally

The external walls of the buildings appear generally in good structural. All heads to doorways and windows to be checked; in all instances, embedded timbers to be removed, voids chemically treated and voids filled with suitable material or lintels (Project Engineer to advise).

External walls

There are repair issues internally related to damaged plaster and wall surfaces as well as moisture decay to embedded timbers. These should be assessed but until the external envelope

of the buildings is consolidated and the cause of internal damage is addressed, the buildings should be allowed a period to dry-out in advance of repairs being undertaken inside.

Roofs

Where individual slates are loose on localised areas of the roof, these can be repaired using lead tingles (strips of lead nailed or screwed to the slate lath above and hooked around the bottom of a loose slate to secure it in place). Use of chemical adhesives to carry out localised repairs to roof slates should be prohibited as it is an irreversible repair and can often render three or four conjoined slates unusable when complete re-roofing is eventually undertaken.

The localised concentration of loose or slipped slates suggests that these are damaged individual slates rather than more widespread corrosion of fixings or 'nail sickness'. For this reason, localised repairs rather than complete re-roofing are the preferable option for ensuring the proper performance of the buildings. However, in relation to the outbuilding, it will be necessary to strip and re-clad the entire roof pitch.

All reusable slates will be salvaged during roof-stripping works with any required replacement or supplementary slates to be approved as matching the existing slates as closely as possible in terms of size, weight, colour and texture both when wet and dry.

Gable parapets and roof abutments

As part of roof repair works, the parapet stones will be carefully lifted and stored in order so that they can be re-laid in the correct sequence on a lime mortar bed. Lead will be removed from the gable wall-tops and the code 3 lead soakers will be installed as the roof is re-slatted.

Rainwater goods

Cast-iron rainwater goods will be taken down, cleaned and repaired where necessary or have replacement sections of matching cast-iron inserted as required. They will then be painted and reinstalled to the correct fall with renewed sealant at joints.

The capacity of gutters and downpipes should be checked if they are adequate for draining the area of roof and the expected conditions of exposure and rainfall. Required replacements should be of cast-iron with a similar profile to any sections being replaced.

The integrity and functionality of ground-level drainage gullies and trenches should be inspected and repaired where necessary to enable storm water to be removed from the buildings and its foundations effectively.

Window sills

Some sills are weathered or cracked leading to moisture ingress which causes external damage to masonry, pointing and internal damage to plasterwork, wall surfaces and embedded timbers. All sills will be inspected for cracks or major weathering leading to unacceptable permeability of these horizontal elements. As appropriate, replacement of heavily decayed sills will be carried out with matching sandstone or else lead flashing, correctly detailed beneath window frames and fixed over existing sills so as to reduce moisture ingress here while causing the least possible damage to masonry.

Window frames

Localised damage and lack of maintenance to timber and metal window frames is leading to decay of windows but also their lack of functionality is preventing the effective use of them to ventilate the buildings' interiors to reduce the conditions conducive to growth of damaging fungal infection.

Proper conservation of timber and metal window frames will ensure the integrity of the window fabric and enable their effective use to adequately ventilate the buildings as required.

The maximum amount of significant historic fabric and detail should be retained and repaired where necessary with replacement only considered where unavoidable. Any replacement should take the original historic detail of existing mouldings and proportions as the reference for new hardwood windows.

A full programme of approved protective painting should be applied to all windows, involving removal of sashes, cutting back built-up or flaking previous layers of paint and application of primer, undercoat and top coats of suitable paint.

Window conservation

Although not directly related to damp issues within the buildings, the historic timber sash windows are in need of repair in order to restore their functionality and original visual contribution to the buildings. While the buildings have been relatively underused for some time, the restoration of the effective, controllable ventilation provided by the sliding sash windows will improve the circulation of fresh air throughout the structures and reduce the occurrence of surface damp issues resulting from condensation of water from stagnant air.

A specialist conservation joiner should be engaged to remove sashes from boxes, strip back built-up thick or loose layers of paint, carry out necessary localised timber repairs to the frames (in approved hardwood) and re-glaze panes where necessary (retaining all re-usable historic glass particularly crown or cylinder glass) before an approved system of protective paint is applied.

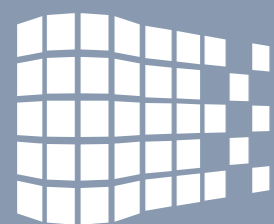
Sash boxes should also be stripped and repaired where necessary before being painted. Stripping of old paint is generally best done with care using appropriate hardened-blade manual scrapers of various shapes and sizes rather than sanding, blasting or chemical treatments. These can result in loss of timber material, sharpness of moulded or carved detail or can cause damage to the surface of the timber rendering it more susceptible to moisture damage or a poor finished painted surface.

Sashes will then be weighed, correctly counterbalanced and hung on new sash-cord with new parting and staff-beads incorporating an approved discrete draught-proofing system to ensure ease of full operation. Where the existing window is a modern timber frame, or where damage to historic fabric through decay or ill-treatment has deteriorated the condition of timber to such an extent that replacement with a new approved hardwood window is necessary, this should be carried out by a suitably experienced conservation joinery works carefully observing the proportions and moulding details of historic glazing bars, mid-rails, sills and horns (or absence thereof). Custom-moulded cutting heads must be obtained to run new hardwood timber elements that match the surviving originals.



GREENORE PORT | VISION 2021

Prepared on behalf of Doyle Shipping Group



McCutcheon Halley
CHARTERED PLANNING CONSULTANTS



EXECUTIVE SUMMARY

Doyle Shipping Group (DSG) acquired Greenore Port in December 2014. The Port is a wholly owned subsidiary of DSG and is Ireland's only privately owned commercial port. The Greenore Port Vision intends to provide an outline of Doyle Shipping Group's existing operation within Ireland and the current and future development of their landholding within Greenore.

DSG has a dedicated workforce of over 600 people and has offices and warehouses in all the major Irish ports, including Dublin, Belfast and Foynes, and owns and operates three private port facilities: Cork Dockyard and Passage West in Cork Harbour and Greenore Port in Louth.

Greenore Port employs 41 people and provides marine and logistical support for sectors including, agricultural feed, construction, renewable energy, livestock exports and manufacturing industries.

Under previous management Greenore Port had suffered a prolonged period of sustained under investment and Greenore Port's immediate priority was to undertake works to improve the neglected infrastructure. To date, DSG have invested €20 million in Greenore Port since taking ownership and envisage investment of up to €50 million as part of this vision.

DSG has been actively involved within the planning process including the consultation process for the National Planning Framework where it encourages strengthening the expansion of existing ports and transportation and infrastructure links, and within the National Ports Policy, DSG will seek the elevation of Greenore Port, from a Port of Regional Significance to Tier 2 status in any forthcoming consultations.

The Greenore Port Vision outlines the current and potential future development of DSG's 23-hectare landholding and outlining The Ports continued focus on Corporate Social Responsibility including the establishment of a Community Gain Fund for local areas. DSG acknowledge the vital maritime role the Port has played in the development of the area, and to celebrate the 150th anniversary in 2023, DSG intend to undertake maintenance works of the Lighthouse and Keepers Cottage and celebrate cultural heritage of the Port by erecting a photographic exhibition on Shore Road.

The reason for producing this vision has been to provide all the Port's stakeholders with a view as to how the Port will be developed over the next number of years. This vision document is a working document that may be amended or adjusted at any time and is non-statutory vision document.



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1.0 DOYLE SHIPPING GROUP IN IRELAND

Doyle Shipping Group (DSG) acquired Greenore Port in December 2014. The Port is a wholly owned subsidiary of Doyle Shipping Group. Greenore Port is Ireland's only privately owned commercial port.

DSG is the largest independent provider of shipping agency services and logistics in Ireland, providing a range of marine services including stevedoring, ships agency, chartering, project cargo terminal management, warehousing, forwarding, ship repair, tug hire and passenger ferries. The company has a dedicated workforce of over 500 people and has offices and warehouses in all the major Irish ports, including Dublin, Belfast and Foynes, and also owns and operates three private port facilities: Cork Dockyard and Passage West in Cork Harbour and Greenore Port in Louth.



FOYNES

Foynes is an integral part of DSG's national infrastructure in the Southwest region. DSG provide a range of marine services including stevedoring and management/distribution of cargo, the company also has 20,000 sq metres of warehousing at their disposal in Foynes.



CORK

The privately owned Cork Dockyard covers an area of c.44 acres and is a major strategic industrial site/port facility within Cork Harbour and includes a range of uses including manufacturing, workshops, storage, stevedoring and marine / port related facilities. Existing on-site infrastructure include 800m of quay wall with deep water berth, a dry dock, roll-on roll-off (Ro Ro) options, 15,000 sq metres of covered storage and 180,000 sq metres of open space for storage and lay-down space. The Cork Dockyard site is identified as one of only three "Category A" ports in the Country with the potential to facilitate off shore / marine energy development.



BELFAST

Reflecting their commitment and presence across the entire island, DSG also have an important base in Belfast where they provide management/distribution of cargo and also have 6,000 sq metres of warehousing at their disposal.



GREENORE

The Port is strategically situated adjacent to the important Dublin- Belfast Economic Corridor, and just 15km from the M1 motorway. The port handles general cargo, project cargo and a variety of commodities including; animal feed, fertiliser, coal and steel. It is the main reinforced steel port in Ireland, capable of handling over 250,000T of steel per annum. The Port handles over 800,000T of dry bulk per annum which consists of animal feed, woodchip, fertiliser, rock and road salt. The Port completed an extensive port dredging project, Greenore Port can accommodate large vessels of 60,000DWT.

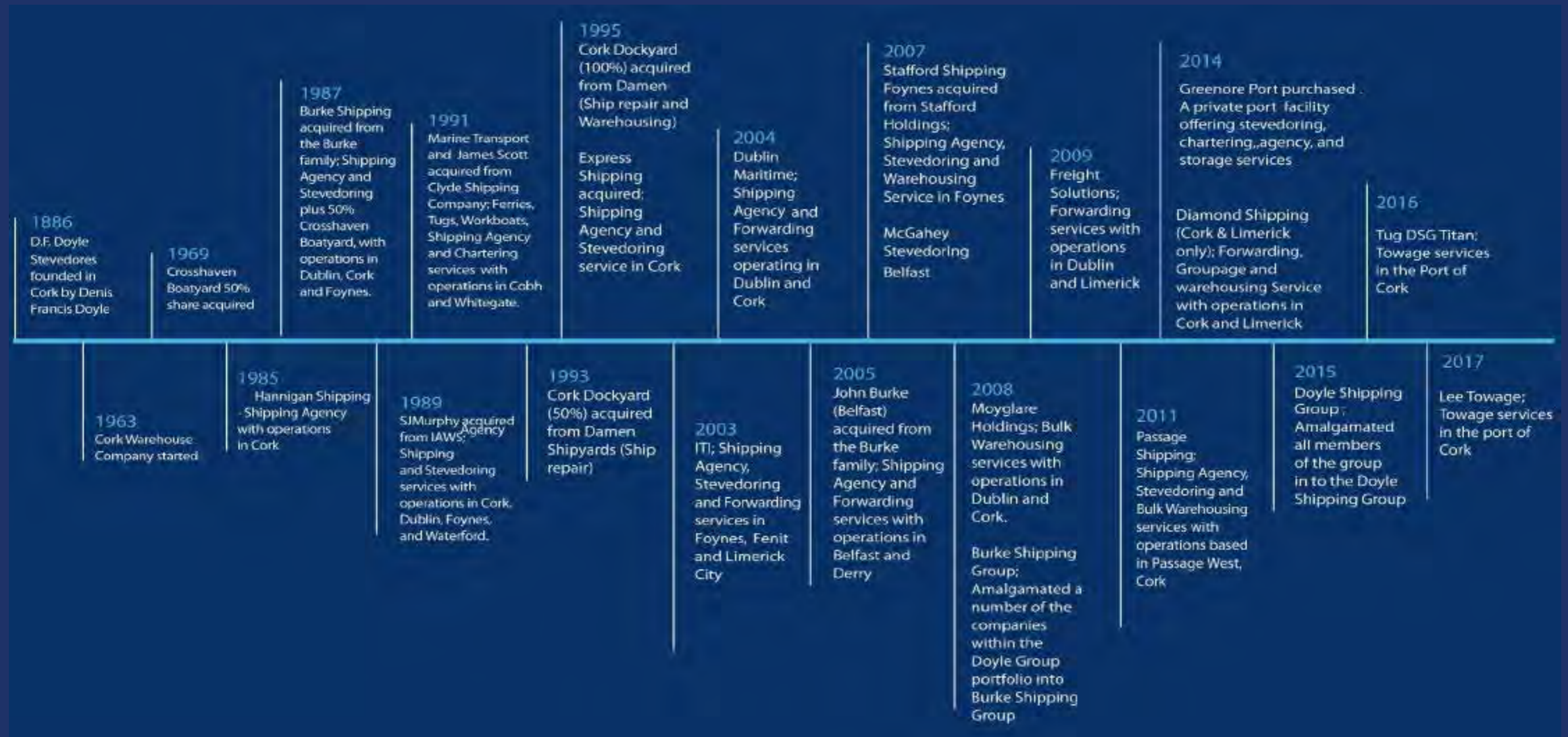


DUBLIN

Dublin is DSG's largest base which operates Ireland's largest container terminal. DSG provides a range of marine services in Dublin including stevedoring, ships agency, chartering, project cargo terminal management, passenger ferries and also have 35,000 sq metres of warehousing at their disposal.



1.1 EVOLUTION OF DSG IN IRELAND



2.0 GREENORE PORT CONTEXT

The Port provides marine & logistical support to the following sectors:



DSG HAS INVESTED €20M SINCE TAKING OWNERSHIP IN 2014 AND ENVISAGE A FURTHER INVESTMENT OF UP TO €30M



GREENORE PORT, CURRENTLY A PORT OF REGIONAL SIGNIFICANCE WILL SEEK TO BE DESIGNATED A TIER 2 PORT OF NATIONAL SIGNIFICANCE WITHIN ANY FORTH-COMING NATIONAL PORTS POLICY



AGRICULTURE



CONSTRUCTION



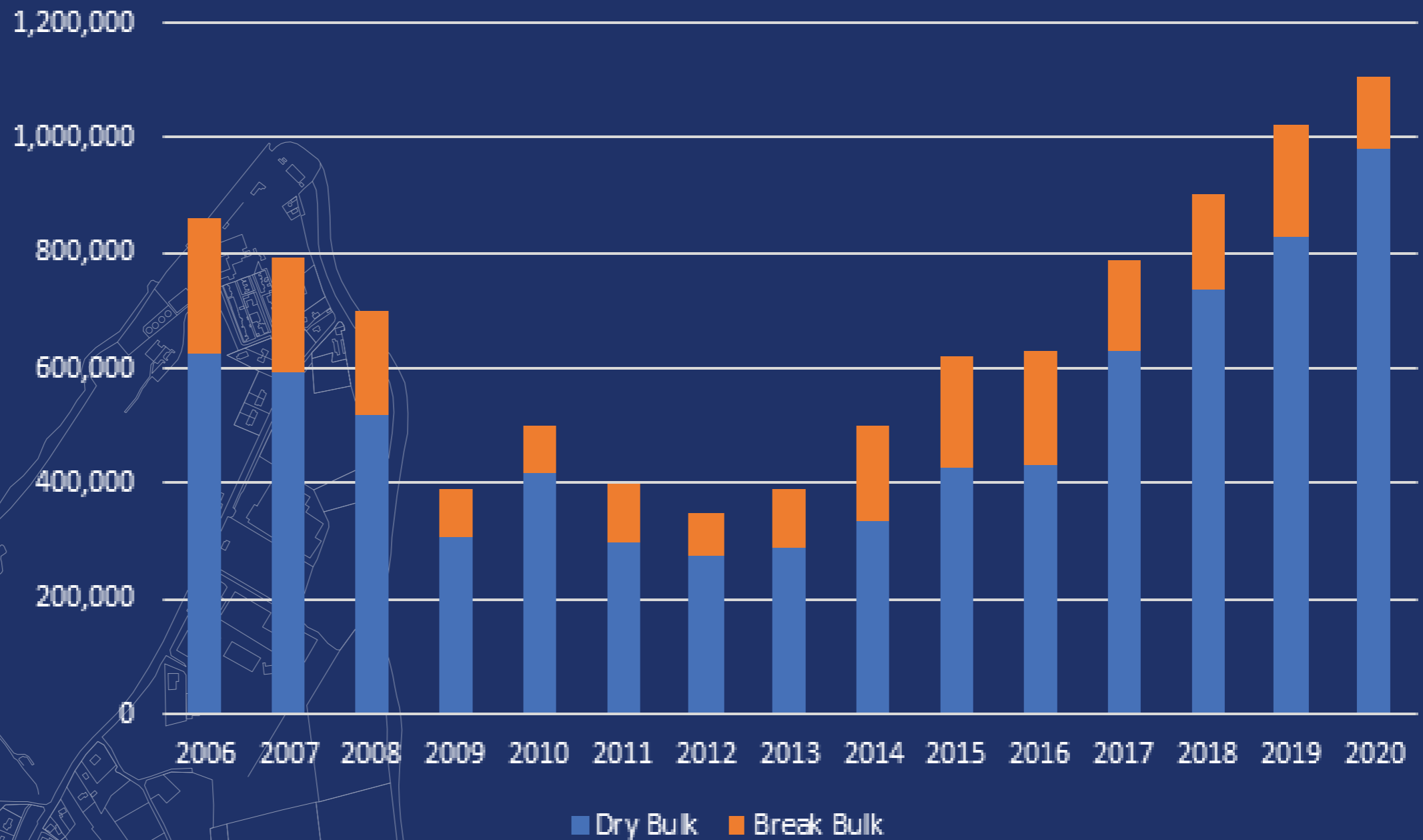
RENEWABLE ENERGY



MANUFACTURING INDUSTRIES



NUMBER 1 PORT IN IRELAND (80% NATIONALLY) FOR REINFORCED STEEL



History of Greenore Port

Greenore Village was established following a nineteenth century commercial venture by the London & North-Western Railway Company (LNWR) who identified the area as a suitable location for a cross-channel ferry service to Holyhead (Wales).

Works began in 1863 and plans included a deep-water port facility with railway connection. The port of Greenore was opened in 1867 and subsequently a ferry service to Holyhead in North Wales was introduced in the 1870's.

The Newry & Greenore Railway opened on 1st May 1873 to connect the LNWR port at Greenore with the town of Newry. A new line which connected to Dundalk was opened in 1876. The route was also envisaged as being attractive to the wealthier classes and in this respect a hotel was built in the 1870's, together with a village for employees and later a golf course. The hotel within the Port has since been demolished.

The LNWR rapidly became the largest joint stock company in the world. Greenore's close link to it was preserved in the names of its streets: Euston Street, its London headquarters and Anglesey Terrace, named after the port town in North Wales where the Greenore mail boat sailed from.

By 1951, a downturn in trade caused by two world wars and the political border of 1923 forced the newly nationalised British Rail to abandon the railway line. The Port was sold into private ownership and re-opened in 1958. From the 1960's both conventional and container traffics were developed. Greenore's container handling facility and ferry services were a pioneering success in the 1960's.

Legal and Planning Context

Within lands under the ownership of DSG, there is two protected structures designated under the current County Louth Development Plan (2015-2021) and listed on the National Inventory of Architectural Heritage (NIAH). They include the lighthouse (LHS009-044), Lighthouse keeper's house (LHS009-045) and Water Tower (LHS009-001). A fragment of wall associated the Greenore Railway Station remains within the Port and is considered to be of some industrial archaeological interest but is not a protected structure.



Lighthouse and Keepers Cottage
Built c.1830



Water Tower
c. 1840

Opportunities

1. Long term term programme for the stabilisation and repair of the lighthouse complex
2. The fragment of the wall associated with the Greenore Railway Station remaining is of industrial archaeological interest and therefore would benefit from a detailed scheme of preservation-by-record so that the heritage significance remains adequately recorded.
3. To celebrate the 150th anniversary of the Port in 2023, DSG intends to undertake maintenance works to the Lighthouse and Keepers Cottage and a photographic exhibition celebrating the cultural history of the Port is proposed to be erected along Shore Road.

Conservation Philosophy and Policy

The fundamental tenet of this masterplan is that key elements of the port area are of heritage significance that should benefit from long term conservation and be managed to maximise appreciation and understanding.

- **Policy 1** In drafting development proposals for these lands, the landowner will be guided by the special character of the site;
- **Policy 2** The landowners will develop proposals for integrating new development into the site while at the same time retaining the heritage significance of the place;
- **Policy 3** Future development within the site should be essentially sympathetic to the historic legacy of the port and the adjoining townscape of Greenore. The aim of such development would be to reutilise and re-invigorate the heritage resource, where practicable and feasible;
- **Policy 4** Any development proposals for the site should seek to respect and conserve the protected structures within the port area, namely the lighthouse and the lighthouse keeper's house. An objective of any long-term development strategy for the port should seek the conservation, refurbishment and the provision of a new appropriate re-use for the building complex;
- **Policy 5** In tandem with any proposed development of the landholding, DSG recognise the heritage, amenity and tourism significance of Greenore and the wider Carlingford Lough area. DSG will seek to be an active stakeholder in measures to promote the attractiveness of this area to visitor and resident alike.

Implementation

As a priority, the landowners will establish a project team to coordinate and oversee the implementation of the plan's policies and associated actions.



As an island nation, our ports are important part of our national economy for the supply of goods and services. Greenore Port supports and benefits local, regional and national economies through their role in creating jobs and transporting goods. The Port is a significant employer in Greenore and the wider region of the Cooley Peninsula, with over 90% of The Port's employees living within 12km of The Port.

It is the goal of Greenore Port to conduct our operations in a professional and socially responsible way, with respect for people and the environment.

The Port adheres to the principle of corporate social responsibility (CSR), considering the interests of all stakeholders, including employees, customers, communities and local authorities.

Since acquiring the Port in December 2014 we have taken many steps to improve our operations:

- In 2016 The Port achieved the ISO management standard for health and safety, environmental and quality.
- It is the policy of the The Port to comply fully with current Environmental Legislation and ISO 14001 standard regarding its processes.

Greenore Port actively engages in charity and local community sponsorship. The Port plays an important role in the society and economy of the region, investing in the development of social infrastructure and cooperates with the local authorities and social institutions.

As part of this 'vision', Greenore Port introduced a community gain fund of €20,000 per annum.

Going forward Greenore Port will actively seek to attract cruise liner business which will have significant economic benefit for the local and surrounding areas, whilst showing off the beauty and history of Greenore, The Cooley Peninsula and Carlingford Lough.



GREENORT PORT TRADING LOCATIONS

NORTH AMERICA	SOUTH AMERICA	EUROPE		AFRICA
CANADA	BRAZIL	GERMANY	LITHUANIA	CAMEROON
USA	ARGENTINA	FRANCE	POLAND	EGYPT
		SPAIN	PORTUGAL	LIBYA
		UNITED KINGDOM	ROMANIA	MOROCCO
		DENMARK	RUSSIA	TUNISIA
		BELGIUM	TURKEY	SOUTH AFRICA
		GREECE	UKRAINE	
		LATVIA	NETHERLANDS	



GREENORE PORT IS THE ONLY DEEP WATER PORT ON THE EAST COAST OF THE REPUBLIC OF IRELAND, OUTSIDE OF DUBLIN



ACCEPTABLE VESSEL PARAMETERS

- 60,000T DEADWEIGHT
- 200M LENGTH
- 8.5M DRAFT
- 33M BEAM



MV THOR MERCURY AT GREENORE PORT IN 2016 (55,000DWT)

2.4 GREENORE PORT KEY FIGURES

	One 51/ Dublin Port		Doyle Shipping Group				
Social	2014	2015	2016	2017	2018	2019	2020
No. Greenore Port Employees	31	36	38	41	42	42	42
Wage Bill	€1.26M	€1.52M	€1.59M	€1.85M	€1.95M	€2.25M	€2.3M
Average Age	49	48	46	44	44	44	44
Total Man Hours	40,025	50,985	57,118	70,397	72,225	72,225	75,300
No. of Haulage Companies Employed by The Port	15	28	29	36	39	42	42
Greenore Port Haulage Bill	€1.55M	€2.15M	€2.69M	€2.95M	€3.6M	€4.1M	€4.5M
No. of Port Warehouses*	4	4	4	4	4	4	4
Employees	10	12	12	14	15	15	15
Trade	2014	2015	2016	2017	2018	2019	2020
Total Tonnage	502,304	620,673	630,498	789,150	905,000	1,024,000	1,105,000
Dry Bulk (agri feed, fertiliser, rock, salt)	334,465	425,861	433,531	631,276	737,000	827,000	981,000
Break Bulk (steel, project cargo)	167,839	194,812	196,967	157,874	168,000	197,000	124,000
Capital Expenditure	2014	2015	2016	2017	2018	2019	2020
CAPEX	€150K	€2.25M	€1.05M	€850k	€3.1M	€7.5M	€5M

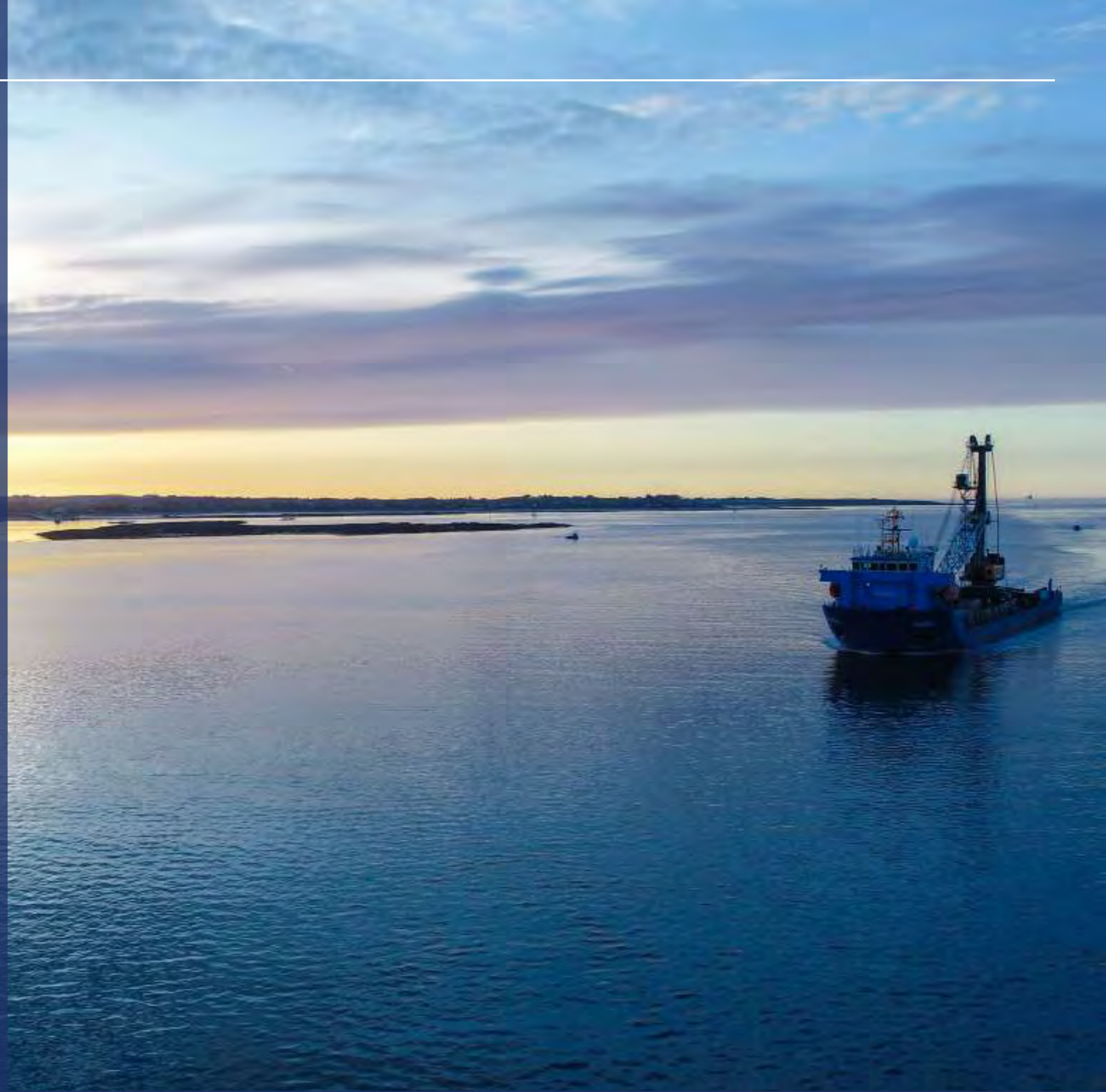
* These warehouses are operated by third party operators

Vulnerabilities

- Economic uncertainty
- Tariffs on agricultural exports
- Physical border
- Ownership of Carlingford Lough boundary
- Downturn in Irish economy

Opportunities

- Strategic location - closest Port to the border;
- Potential for commodities such as agricultural feeds currently imported through Northern Ireland to be redistributed to Greenore;
- Increased taxes for state (import duty etc.).



LOUTH COUNTY DEVELOPMENT PLAN 2015-2021

The following extracts from the CDP are considered relevant.

Policy TC 41 - To support the development and expansion of the ports of Drogheda, Greenore, Dundalk and Clogherhead subject to the preparation of a masterplan and compliance with all relevant EU policies such as Water Framework, Habitats, SEA and EIA Directives.

Policy TC42 - To implement the recommendations contained within the National Ports Policy 2013 as they relate to the ports of County Louth.

Section 6.1 - The M1 also provides rapid access to international airports at Dublin and Belfast. Additionally, Louth boasts three commercial freight ports, namely Drogheda, Dundalk and Greenore.

Section 5.11 outlines policies applicable to Greenore Village's status as an Architectural Conservation Areas (ACA's). Policies HER 45-52 are considered relevant to proposals within the periphery of the Greenore ACA.

NATIONAL PORTS POLICY (NPP) 2013

Establishes a framework for the future sustainable development of the commercial ports sector in Ireland. Formulated policy aims to increase the contribution of marine sector to the overall economy, recognising the relationship between spatial planning and ports. The core objective of the NPP is to facilitate competition (inter-port and intra-port), delivering an effective market with appropriate services.

A key aspect of the NPP is the provision of adequate and efficient port capacity into the future, which must respond to global requirements, the most notable of which is identified as trends towards larger vessels, driven by economies of scale and the shipping industries response to environmental regulations.

Action

- Greenore identified as Port of Regional Significance. It is Doyle Shipping Group's intention to seek an elevation to Tier 2 status in forthcoming policy consultations to allow Greenore Port to make a full contribution to facilitating economic recovery and prosperity

BORDER REGIONAL AUTHORITY PLANNING GUIDELINES 2010-2022

Greenore is identified as an 'excellent deep water port that may have future potential for increased commercial activity'.

Policy supports the provision of adequate port facilities for commercial, fishing and amenity purposes within the region subject to compliance with the Habitats Directive (INFP14).

Greenore forms part of the newly established Eastern and Midland Regional Assembly.

NATIONAL PLANNING FRAMEWORK

- The recently published National Planning Framework (NPF) recognise ports as enablers of economic growth. As an island nation, we depend on the quality and efficiency of our ports to a far greater extent than many of our trading partners. The NPF states that to maintain economic growth, additional port capacity must be delivered in a timely and predictable manner.
- The NPF highlights that the long-term international trend in ports and shipping is toward increased consolidation of resources to achieve optimum efficiencies of scale. This has knock-on effects in terms of vessel size, the depths of water required at ports and the type and scale of port hinterland transport connections.
- As part of Ireland 2040, the Government wishes to capitalise upon and further support and promote the inherent economic potential of the corridor, building upon existing strengths. The continued growth of Greenore would contribute to this aim; the port benefits from naturally occurring deep water and is located proximate to the Dublin-Belfast Economic Corridor.

MARINE SPATIAL PLAN 2021

Both ports and shipping have to interact with new activities that redefine the use of marine for example offshore energy installations and aquaculture. This creates new issues in terms of safety, access to space, environmental concerns and the efficiency and competitiveness of maritime transport.

With the entry into force of the EU Directive for Maritime Spatial Planning (MSP) in 2014, MSP is becoming an increasingly important policy framework to analyse and organise human activities in marine areas to achieve ecological, economic and social objectives'.

Ireland has transposed the Directive through the European Union (Framework for Maritime Spatial Planning) Regulations 2016. The regulations establish the legal basis and broad framework for Ireland to implement MSP through the development of a national Marine Spatial Plan by 2021.

Land-sea interactions are required to be considered when preparing the Marine Spatial Plan and DSG as owners of Greenore Port will be engaging with the plan preparation through the consultation processes.



3.1 GREENORE ZONING

Coastal Erosion & Flood Risk

Irish Coastal Protection Strategy Study

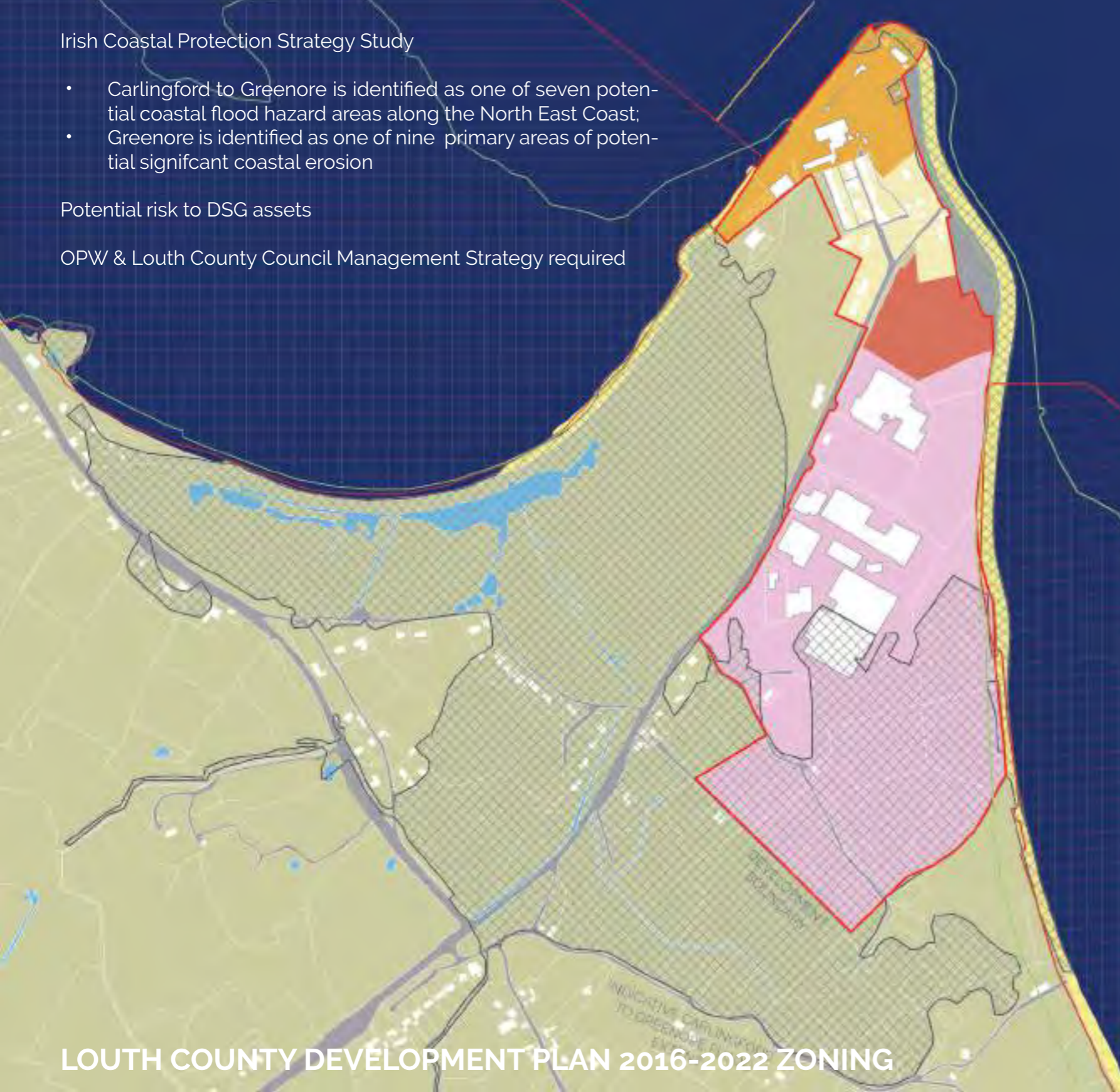
- Carlingford to Greenore is identified as one of seven potential coastal flood hazard areas along the North East Coast;
- Greenore is identified as one of nine primary areas of potential significant coastal erosion

Potential risk to DSG assets

OPW & Louth County Council Management Strategy required

National Development Plan 2018-2027

- Government's commitment to delivering flood relief schemes to minimise the impact of flooding
- Carlingford & Greenore: €24 million scheme to protect 296 properties
 - ✓ DSG recognises the risk from sea level rise and increased storm frequency associated with climate change
 - ✓ DSG recognise the role land management has in shaping the causes and impacts of flooding
 - ✓ DSG will seek to avoid inappropriate development in areas identified as at risk of flooding
 - ✓ DSG will incorporate best practice design measures into development proposals to future-proof drainage assets
 - ✓ DSG will engage with the Office of Public Works (OPW) during the scheme development process



- Development Boundary
- ▨ Carlingford Lough SPA
- ▨ Carlingford Shore SAC
- ▨ Flood Zone
- Port/Port Related Activity
- Development Area
- Industrial/Residential
- Industrial

This map provides only an indication of areas that may be prone to flooding. They are not necessarily locally accurate and should not be used as the sole basis for defining Flood Zones, or for making decisions on planning applications. Flood Zone boundary illustrated is based on the Greenore Composite Map (Land Use - Flood Extents) within the Louth County Development Plan 2015-2022 and Carlingford to Greenore Predictive Flood Extent Maps within the North Western Neagh Bann CFRAM Study

LOUTH COUNTY DEVELOPMENT PLAN 2016-2022 ZONING



TOTAL LANDHOLDING: 23 HECTARE

AERIAL PHOTO TAKEN IN 2015

GREENORE PORT RECENT PROJECTS AND DEVELOPMENTS

PLANNING APPLICATION SUBMITTED FOR ADDITIONAL STORAGE AREA APRIL 2018

EXTERNAL STORAGE AREAS

WOODCHIP YARD

CONSTRUCTION OF ARKADY SILOS COMMENCED

NEW 420T CRANE

NEW 100T CRANE

OPEN HYDRO CAR PARK

OPEN HYDRO

SCENIC CARLINGFORD FERRY

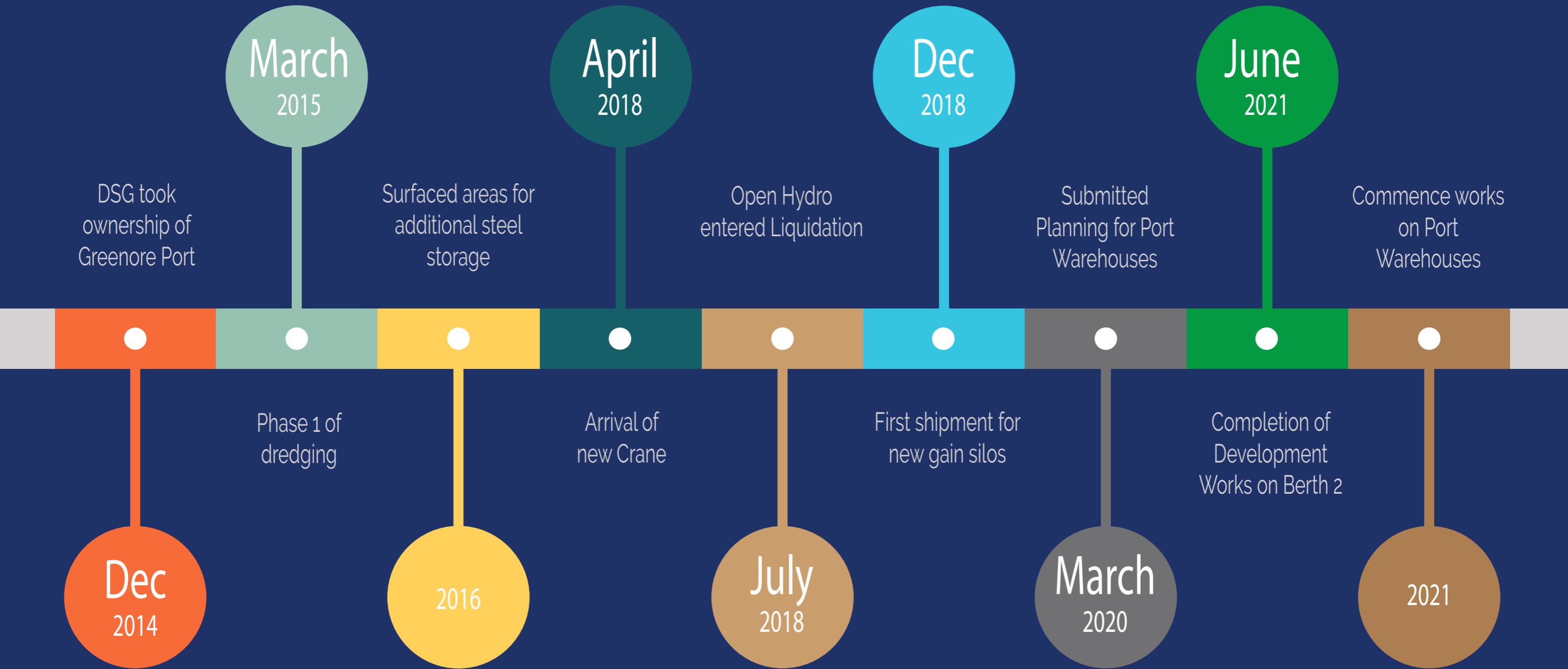
ANCHORING WORKS

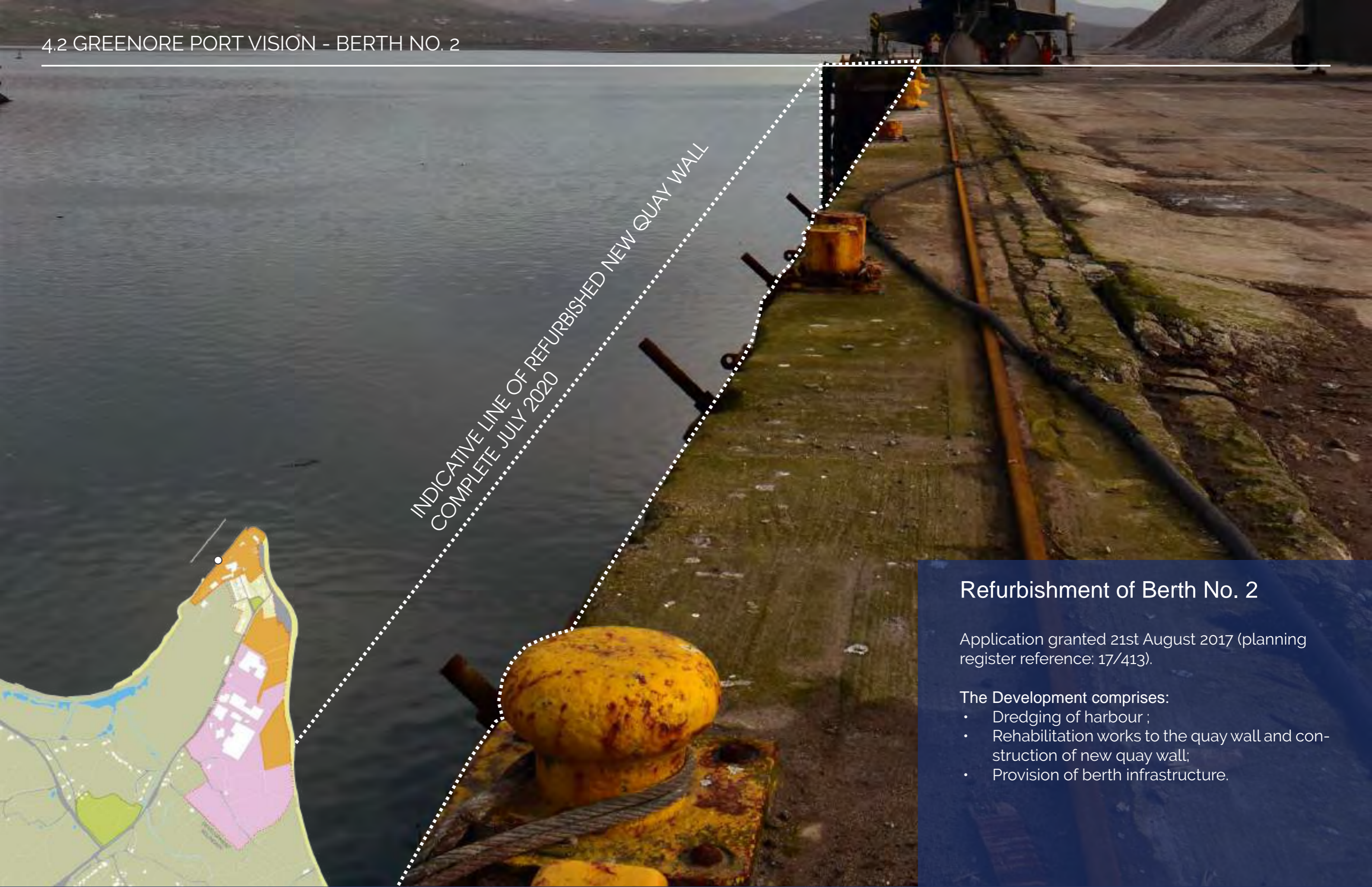
PLANNING GRANTED FOR REFURBISHMENT WORKS ON BERTH 2

ROCK DREDGING

The Bog

GREENORE PORT TIMELINE





INDICATIVE LINE OF REFURBISHED NEW QUAY WALL
COMPLETE JULY 2020

Refurbishment of Berth No. 2

Application granted 21st August 2017 (planning register reference: 17/413).

The Development comprises:

- Dredging of harbour ;
- Rehabilitation works to the quay wall and construction of new quay wall;
- Provision of berth infrastructure.



Grain Silos

2 No. Grain Silos

Granted permission under planning register reference: 16/842 on 15th May 2017.

Development to comprise the following:

- 2 No. grains silos;
- 1 No. bulk reception unit;
- 1 No. grain removal building.

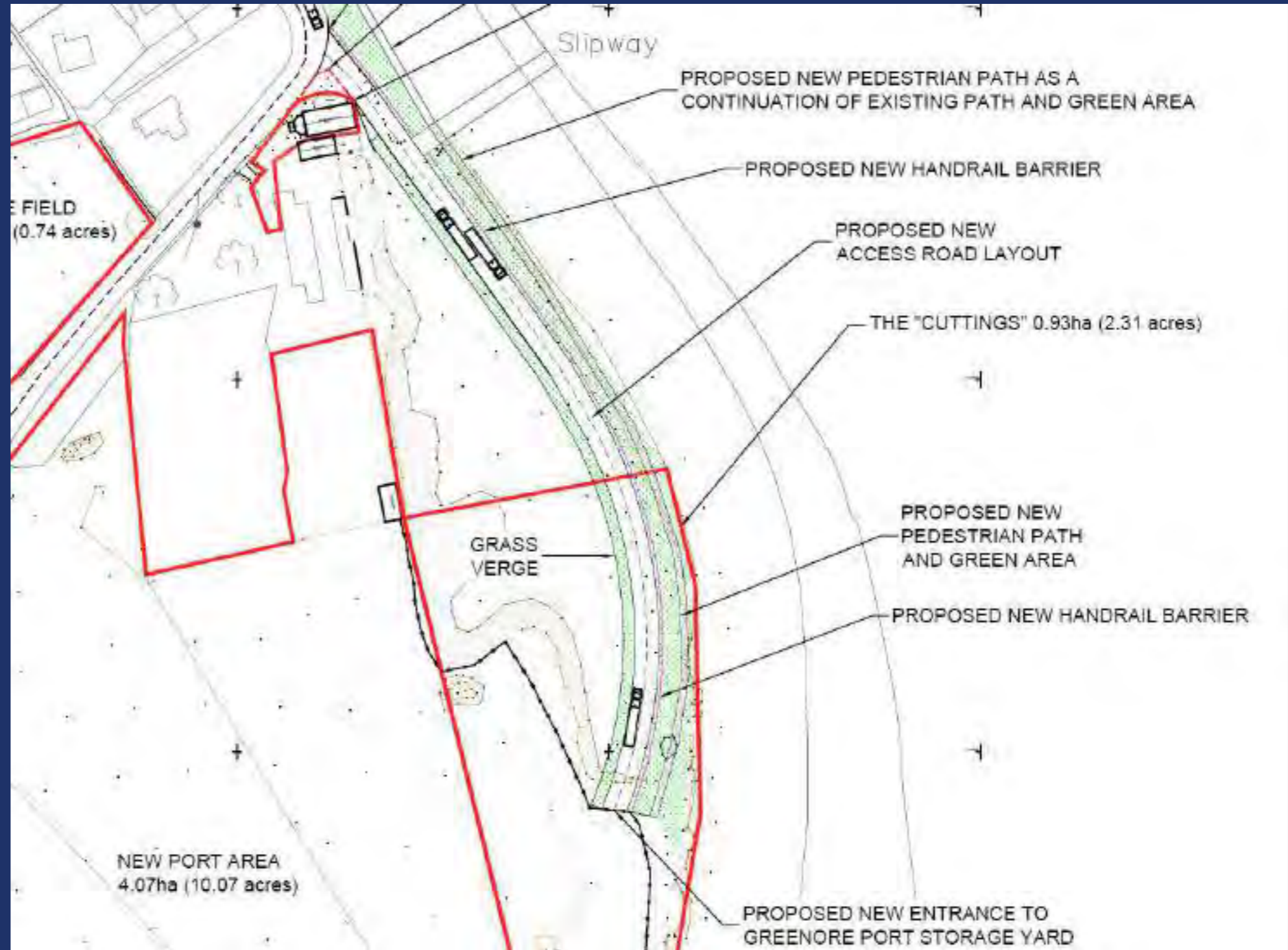


Warehouse Expansion

The roof of the current warehouse at the port to be raised and extended as per picture. On completion of the works, the port will be able to store products such as animal feed, fertiliser, peat, steel and timber in close proximity to quay wall. This will make the port more competitive with other ports who have covered storage adjacent to their quay walls. There is also a growing need for covered quay side storage because some of the major ports such as Dublin Port, are no longer permitting warehouses to be built near the quay walls.

New Road

Proposed new road to Port storage areas, to mitigate traffic going in close proximity to the residential part of Greenore.





Potential Berth 3

- In 1990, a previous owner of the Port at Greenore, Aodhagan O'Rahilly set out to create a third berth at the Port. Due to financial reasons the berth was never complete and today 80m of caissons sit unused adjacent to berth 2.
- With a third berth the Port would have the capability to accommodate 3 small vessels at one time or else two large vessels side by side.

Potential Ro-Ro and Lo-Lo Facility

In the Celtic Tiger when there was plans to relocate Dublin port outside of Dublin, Greenore Port was purchased by investment company One51 and Dublin Port Company. They set out to construct a Ro-Ro and Lo-Lo facility on unused lands adjacent to Greenore Port.

Along with the 9 hectares of an unused land bank, their intention was to reclaim 5 hectares on the foreshore and create 300m of linear quay length with Lo-Lo berths for containers and a Ro-Ro ramp to cater for short sea ferries.

There are no immediate plans to reconsider this project but will be looked at if future market trends demand it.





SITE OF 0.4 HECTARES

The Bog

Additional Open Storage

- DSG have recently acquired a site measuring 0.4 hectares along the Shore Road, which adjoins the existing Greenore Port landholding.
- It is envisaged that the site will be developed to provide additional open storage, with a view to addressing the existing shortfall in available storage capacity.



40 Acres of Open Storage Area

Office and Car Parking Facilities

Warehousing

Berths for O&M Vessels

RO-RO Berth

Potential Renewable Energy Handling & Storage Facility

Along with 9 hectares of a landbank currently owned by The Port, it would be the intention to reclaim a further 5 hectares on the foreshore and create a 300M linear quay length along with a further berth of 140M. The draft of this berth is proposed to be up to 14M in depth.

4.10 GREENORE PORT VISION - RENEWABLE ENERGY OPERATIONS AND MAINTENANCE FACILITY



4.11 GREENORE PORT VISION - FLOOD RISK MANAGEMENT

Food Risk Management for County Louth

The Catchment Flood Risk Assessment and



Management (CFRAM) programme, is a flood risk study undertaken for areas believed to be at risk of flooding. The CFRAM programme culminated with the launch in 2018 of Flood Risk Management Plans (FRMP), which proposed outline flood relief projects.

The proposals for Greenore are contained within the Neagh Bann Flood Risk Management Plan. The Plan references the Louth Coastal Erosion Study completed in 2016, following significant coastal erosion since 2010 at Greenore and included coastal erosion, flood mapping and a coastal erosion risk management plan identifying the measures to manage the risk. It states that Louth County Council

intend to move to the next stage, to provide the Coastal Erosion Defences identified in the Study.

The FRMP establishes that the potentially viable flood relief works for Carlingford and Greenore that may be implemented after project-level assessment and planning might include physical works. The proposed measures consist of hard defences, pumping stations and improvement of channel conveyance at the locations identified in the plan below. The estimated preliminary cost for the proposed flood relief scheme is €23.41m.

The proposed measure will deliver several key flood protection benefits; reducing risk to numerous local properties and commercial properties, NIAH buildings, transport links, agricultural land and social infrastructure/amenity sites in the medium and long term.

As is illustrated in this plan, specific measures at Greenore Port are not proposed. However, it is noted that the measures included in the FRMP are not definitive and final, and as part of the project-level assessment required to progress the project to planning, more detailed assessments are required at a local level and further public and stakeholder consultation will be undertaken.

A dedicated project office, the Louth Flood Relief Projects Office, has been established to progress the proposed scheme and Doyle Shipping Group are actively engaging with them to ensure that conflicts between the ports future development proposals and any planned flood relief works do not conflict.

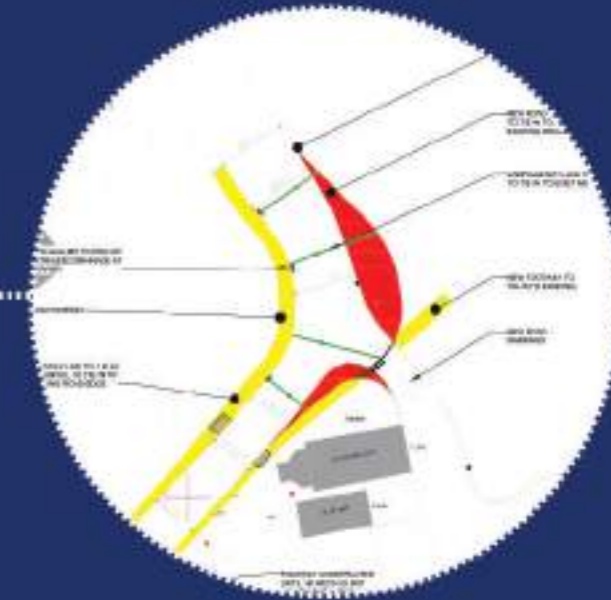


4.12 GREENORE PORT VISION - OTHER INFRASTRUCTURE

TO BE SUPPLIED BY DSG AND OTHER PROVIDERS WITHIN THE AREA



Road Scheme on Shore Road - Compliance with Planning Permission for Ferry Terminal development - Frazer Ferries (ABP ref. PL15 .243191/13/241)



Potential New Entrance to Open Storage Areas (left-in only): It is intended to provide a left turn only access to open storage areas thereby enabling movement of goods from berth to storage areas in a safe and efficient manner.

Road Scheme is to be undertaken to widen the corner on Shore Road

Carlingford Scenic Ferry: The ferry service between Greenore and Greencastle began operation in June 2017 with a service everyday from 7:30am to 9:30pm.

GREENORE PORT DEVELOPMENTS



- CARLINGFORD SCENIC FERRY
- LIGHTHOUSE & KEEPERS COTTAGE
- POTENTIAL NEW PORT ENTRANCE
- SHORE ROAD WIDENING
- BEACH HOUSE & CAFE
- HORSE FIELD - POTENTIAL AMENITY USE
- POTENTIAL FUTURE HARD-SURFACE AREA
- POTENTIAL NEW PORT AREA (SID)
- POTENTIAL NEW ENTRANCE ROAD TO OPEN STORAGE AREAS

- BERTH 2 REFURBISHMENT
- 2 GRAIN SILOS
- OPEN HYDRO EXPANSION
- POTENTIAL BERTH 3 EXPANSION

OPEN STORAGE AREAS

OPEN STORAGE AREAS GRANTED UNDER:
 PL15.246093 (COUNCIL REG. REF: 15/496) &
 PL15.246577 (COUNCIL REG. REF: 16/114)
 APPLICATION FOR ADDITIONAL STORAGE
 SUBMITTED TO LCC APRIL 2018 (18/285)

COAL YARD

PROPOSED USE: OPEN STORAGE OF
 BORD NA MONA WOODCHIP

THE BOG

POTENTIAL NATURAL FLOOD ATTENUATION AREA