

Urban Design Framework Plan for The Heritage Quarter, Drogheda



Údarás Áitiúla Lú
Louth Local Authorities

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Prepared by

Shaffrey Associates Architects - Mitchell Associates - Stephen Little Associates - Margaret Gowen

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Fig. 1.1 Skyline view of Drogheda from Millmount

1.0 Introduction

Drogheda is a town of recognised urban quality, being amongst the more architecturally significant towns in the country. This is formally acknowledged in the Drogheda Borough Council Development Plan 2011-2017 which identifies the Heritage Quarter of Drogheda as a distinctive character area comprising and extending beyond the Town Centre.

Drogheda shares, with Dundalk, the status of Ireland's largest town, with a current population of 30,393 for the legal town of Drogheda and 38,578 for the legal town and environs (including the southern environs in Co. Meath)¹. This represents an increase of almost 5% since 2006.

With a significant population (750,000) within close proximity (20km) and located on the Dublin - Belfast (Eastern) corridor, well served by public and private transport routes, Drogheda is likely to (re)-experience development pressure in the mid to long term. Therefore, while this Plan has been prepared during times of great social and economic uncertainty, it aims to take a long term view which is optimistic with regards to Drogheda's continued significance in the regional context.

1 CSO 2011 population statistics for Drogheda

The challenge which such a future scenario presents is how this development can take place in a manner which supports the continued vitality and primacy of the town centre/heritage quarter while maintaining, enhancing and extending its recognised architectural and urban qualities.

In tackling this challenge in the context of an urban design framework, two key questions arise:

- what sort of town does Drogheda aim to be?
- how can Drogheda change and grow and still retain the very particular urban qualities for which it is famed?

This Plan seeks to address these questions and provide guidance as to how an historic town can evolve to meet the needs of contemporary and future citizens in a manner which makes the most of its positive qualities and assets.



Fig. 1.2: aerial photograph of Drogheda town centre

1.1 Context and Aims of Framework Plan

The Urban Design Framework Plan is a direct policy action of the Drogheda Borough Council Development Plan 2011-2017:

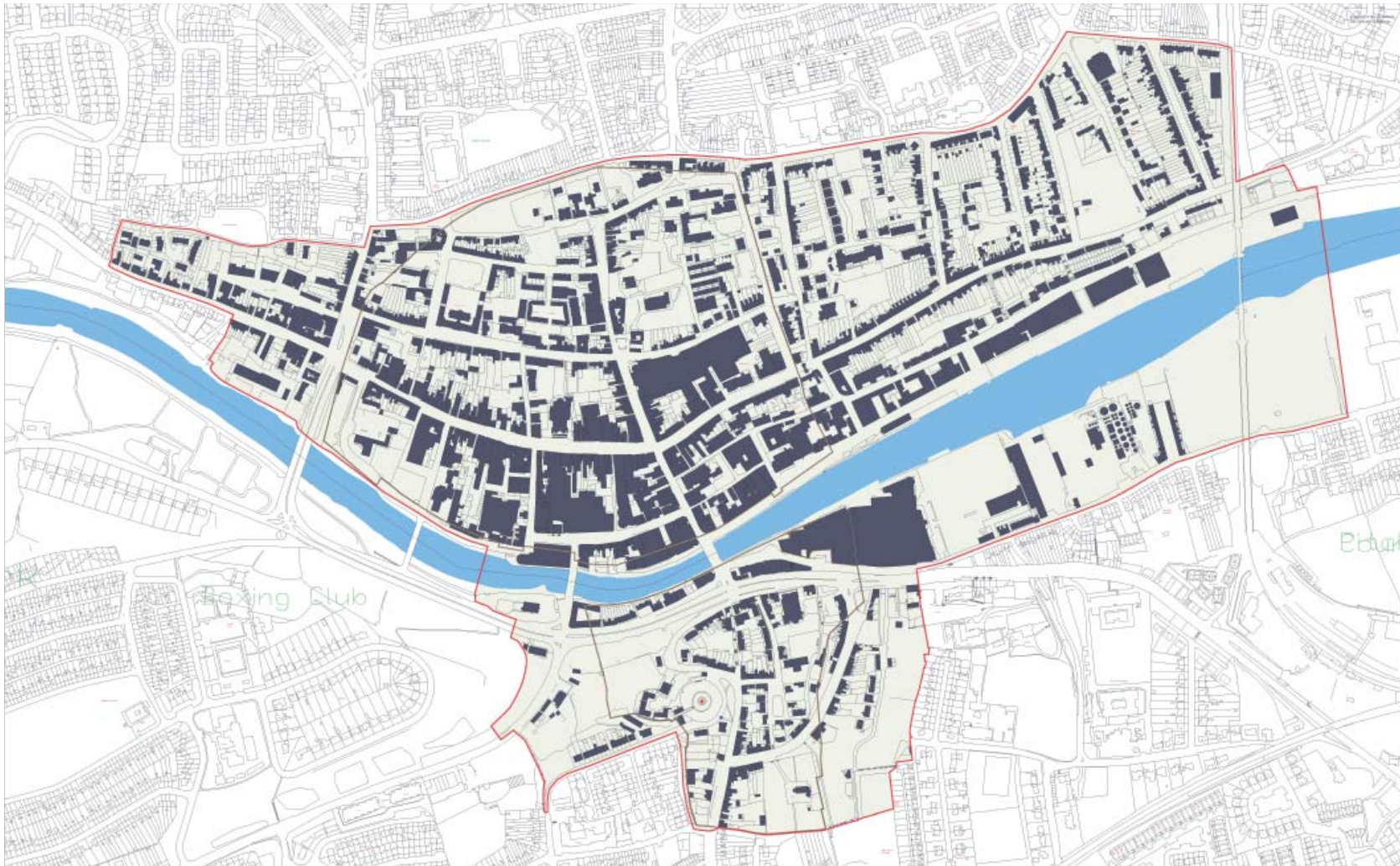
Policy TC1:

To prepare a detailed Urban Design Framework Plan for the Town Centre within the context of the wider Heritage Quarter.

This Urban Design Framework Plan, commissioned by Drogheda Borough Council/ Louth County Council provides urban design guidance for development within the

Heritage Quarter. It will support existing statutory policy and guidance and will be used to assist in development control as well as to direct and guide those considering and preparing development proposals within the Heritage Quarter.

The Urban Design Framework Plan also sets out a vision for how Drogheda can evolve as a significant urban centre, with durable and enduring buildings and places which are pleasant to spend time in and is active, diverse and culturally rich. The plan looks at a range of approaches to realising the potential of the existing built and spatial infrastructure as well as opportunities for new development of varying scale and typology.



Map. 1.3 Map showing the Heritage Quarter shaded and outlined in red. This is the area to which the Urban Design Framework Plan applies.

The area designated as the Heritage Quarter is outlined in Figure 1.3 and comprises the urban lands within the line of the historic town walls, extending beyond this, to the east including lands north and south of the river to just east of the Boyne viaduct and, to the west on the northern side of the river comprising a smaller area of settled residential and former industrial lands.

Much of the lands within the Heritage Quarter support well established uses - such as the large areas of settled residential communities east and west of the Plan area and south, in the Millmount quarter. Elsewhere, particularly within the town centre and along the River, there has been greater change in recent years, including

significant new development and, more recently, greater vacancy, dereliction and underuse. Based on an analysis of the prevailing townscape characteristics within the study area, the Urban Design Framework Plan sets out a range of strategies towards enhancing and protecting the positive qualities which also guide and enable development and change in a managed way.

The Plan will be implemented through numerous projects and initiatives of varying scale and nature and, by a range of bodies - local authority, national and state organisations; voluntary groups; professional developers and, individual property owners. All citizens of Drogheda can influence its success or otherwise.

1.1 Layout of Report

Following this introductory chapter, the report is set out in seven sections which can be broadly categorised into Context and Strategy, as follows:

CONTEXT:

- Section 2.0 provides a brief, but important, description of the distinctive architectural and urban qualities of Drogheda, using analytical maps to highlight some of the more distinctive and significant elements which the Urban Design Framework Plan aims both to protect and promote as a positive urban enhancement strategy.
- The outcomes of the consultation process are set out briefly in section 3.0. This is supported by Appendix A which provides a report on the stakeholder and public consultations held.
- The relevant statutory and policy framework which underpins and guides this Plan is set out in section 4.0. So, while this includes the statutory Development Plan and LAP documents, it also identifies non-statutory guidance and policy - national and local - which informed the plan.
- Townscape Analysis is addressed in section 5.0, primarily using analytical maps and illustrations. This section looks at strengths and weaknesses within the town, so that the Framework Plan Strategy can be targeted to support the positive aspects and improve the weaker aspects.

STRATEGY

- Section 6.0 contains the Framework Plan. At the outset a number of key objectives are provided (6.1), effectively setting the 'vision' which sits behind the Framework Plan. The vision and objectives have evolved from the consultation process, the review of relevant plans, policies, etc, and the townscape analysis. It is an impor-

tant section in that it underpins all the guidance within the Framework and sets goals against which the strategies and implementation measures can be assessed.

- The Framework Plan provides guidance in terms of general policies and strategies under 5 principal headings:
 - 6.2 Existing Buildings
 - 6.3 New Development
 - 6.4 Living in the Heritage Quarter
 - 6.5 Scale of Development
 - 6.6 Public RealmUses and activities are considered within each of the above.
- The general policies within section 6.0 also address scale of new development within the Heritage Quarter. It does this by using north-south cross sections which illustrate the existing profile and the influence of the town's topography on this. The scale strategy relates to the prevailing characteristics of Drogheda and the distinctive spatial sequences described in section 2.0
- Section 6.0 then goes on to illustrate, through diagrams, how the key principals within the guidance might be applied in specific locations within the town. These locations have been selected for a range of reasons which are explained in this section
- Section 7.0 addresses implementation. It is recognised that there will be a range of methods, players and drivers influencing when and how the plan is implemented. The main purpose of the Plan is to provide guidance and, hopefully, some inspiration, to the full spectrum of potential urban actors. This section also sets out a range of potential implementation measures, which may be useful in pump priming or leveraging the kind of development activity this Plan

supports. Some of these measures reflect the particularly challenging times for historic urban centres which exist presently and, so, may be appropriate to be time-based if considered

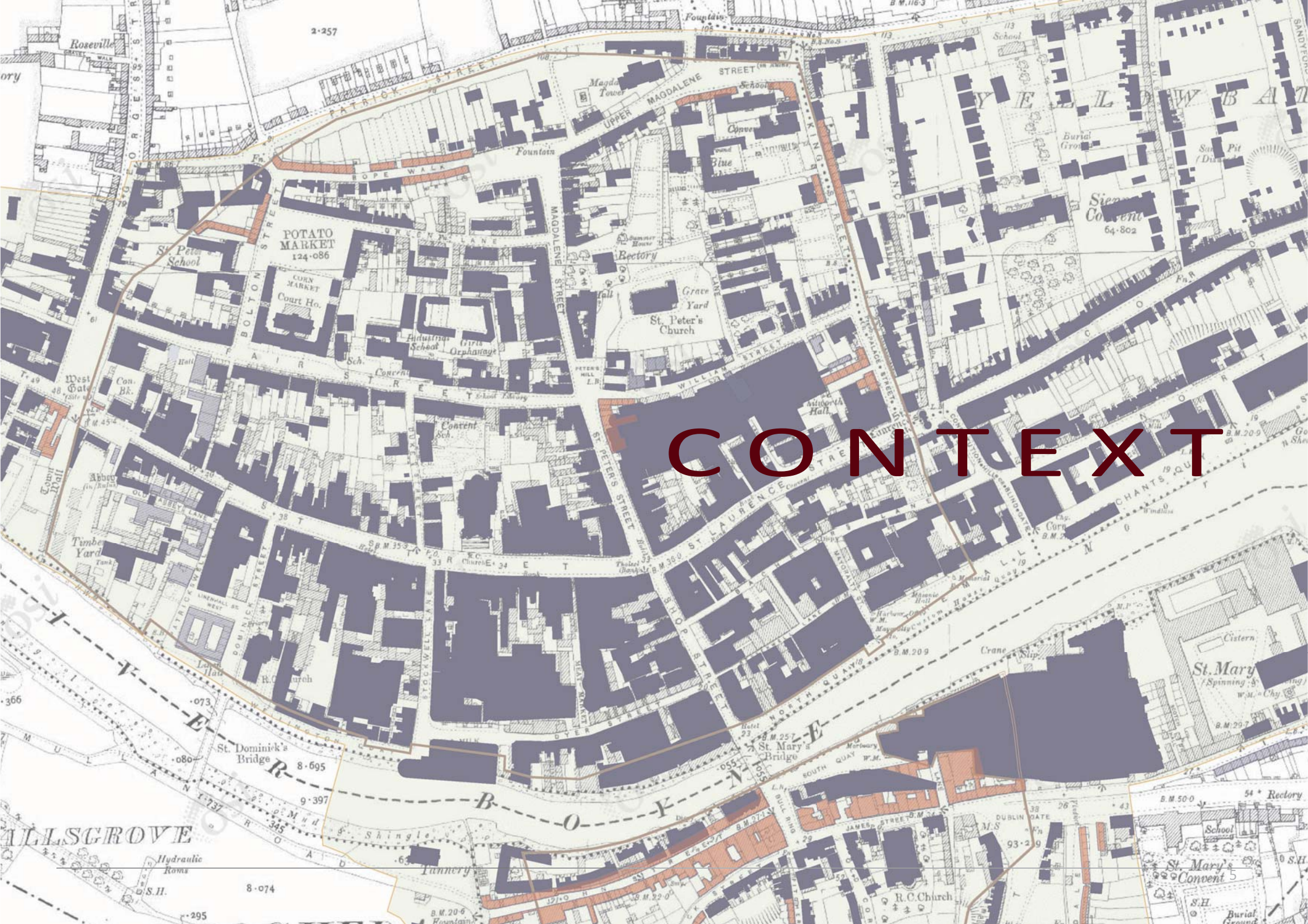
1.2 Acknowledgements

This Plan is a result of many generous and helpful contributions which is greatly appreciated, in particular the following:

- Steering Group
- Drogheda Borough Council
- Louth and Meath County Councils
- Barlow House
- All those who participated in the stakeholder and public consultation workshops
- Drogheda Port Company
- Office of Public Works
- Department of Arts, Heritage and the Gaeltacht
- WSP Consultants

1.3 Consultant Team

Shaffrey Associates Architects
Mitchell Associates Landscape Architects
Stephen Little Associates Planning Consultants
Margaret Gowen, Archaeologist



CONTEXT

ALLSGROVE

8-074

295

B.M. 20-6

B.M. 257

B.M. 209

B.M. 500

54

S.H.

Burial Ground

2.0 Drogheda: Architectural and Urban Qualities

The strong presence of the historic urban form and structure of Drogheda has persisted over time and remains a dominant and distinctive part of its physical character. This is underscored by the many historic landmark structures and their location - for example, the church spires; Millmount Tower on its grassy mound; Magdalene Tower, St Laurence's Gate and the Abbey Transept Tower all part of a rich medieval inheritance and, the Boyne Viaduct.

While it is the striking landmarks that most people often immediately notice in any town, the urban structure and form (its shape and the character of streets and squares) is the frame which people experience as they move through a place and by which they negotiate and understand a town. Where this is weak, the experience at street/ground level can be poor and unpleasant. Where it is strong and distinctive, it becomes enjoyable and memorable. Drogheda possesses a strong urban structure and form.

This quality of form must be understood as an asset which has value beyond that of historic artefact but also as a fundamental component of good urban place making for now and the future.

The endurance of urban structure can be seen through studying historic maps and views of Drogheda. From the wonderful Barnaby Goche representation of 1574 (Fig. 2.1) - which shows a remarkably sophisticated town form - to the 1911 edition Ordnance Survey map (Fig. 2.8), the clarity of the principal elements of Drogheda's urban structure is striking.

By 1574 the walls, the great north-south spine running from the Friary (Magdalene's Tower), to Millmount, crossing the river at St. Mary's Bridge,

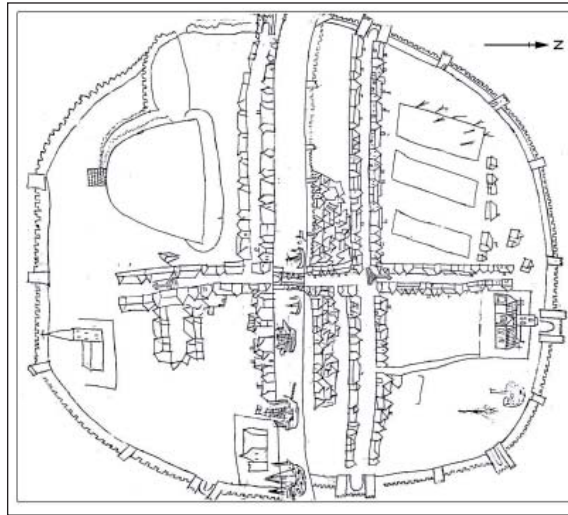


Fig. 2.1 Barnaby Goche Map 1574 (note direction of north point)

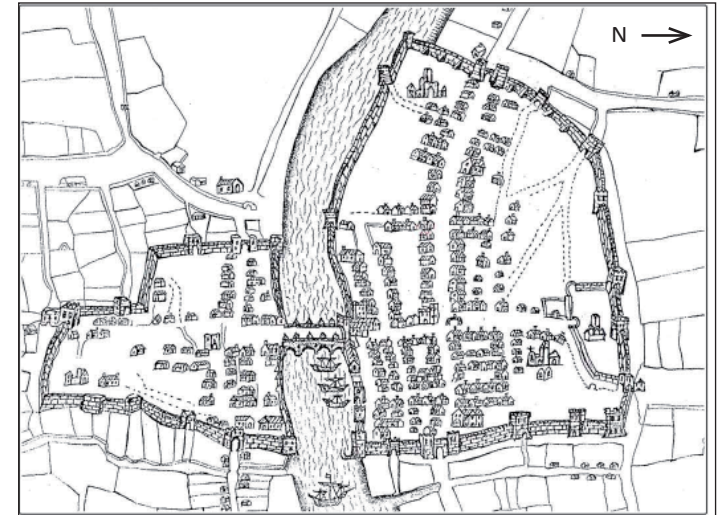


Fig. 2.2 Newcomen Map 1657 (north point as per Goche map)

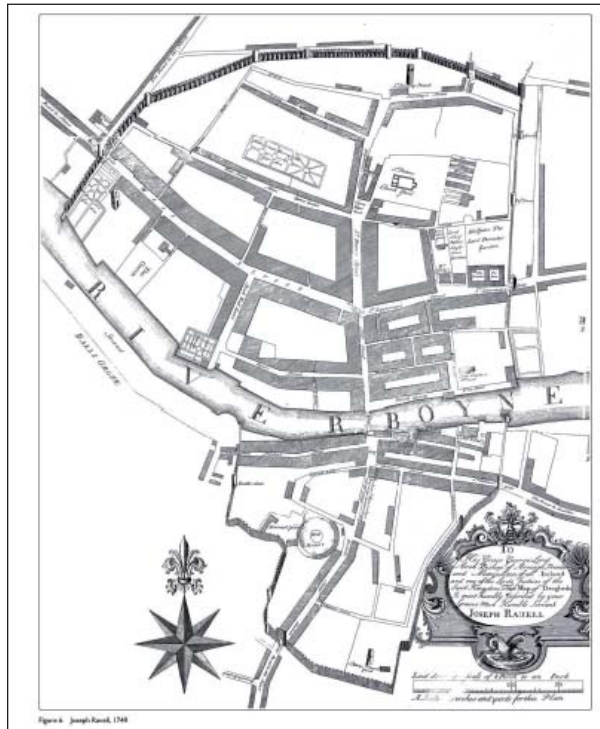


Fig. 2.3 Ravell Map 1749



Fig. 2.4 Van Hagen, 1718



Fig. 2.5 Ricciardelli c. 1753

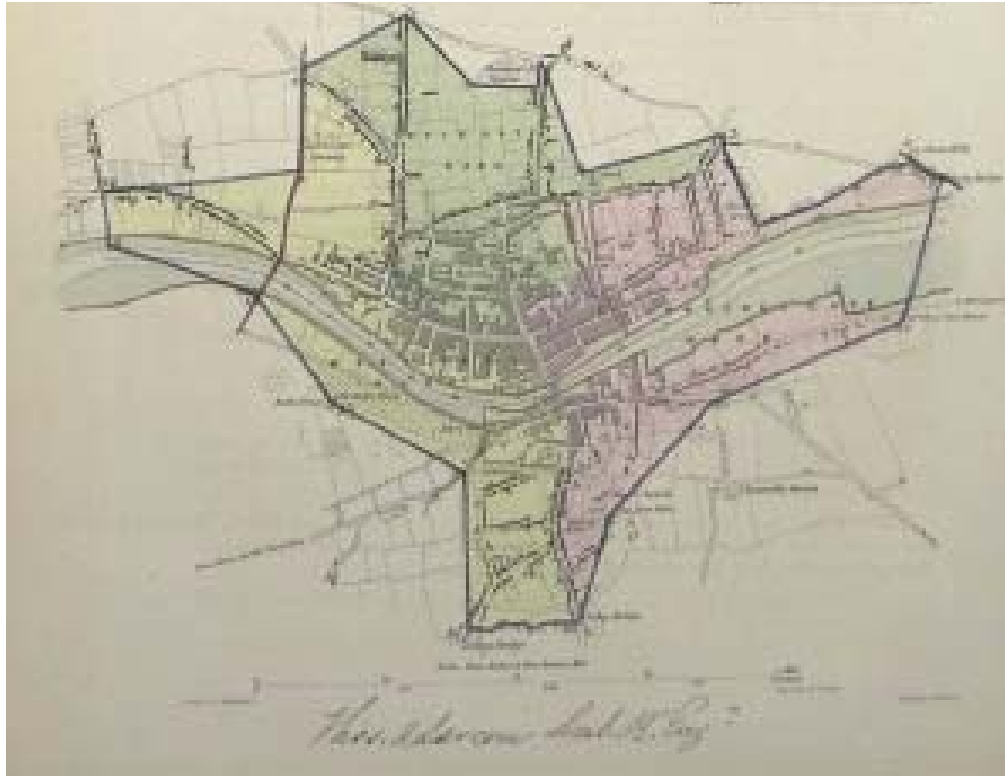


Fig. 2.6 Larcom Map. 1837



Fig. 2.7 1837 edition, Ordnance Survey

(approx.), and the east-west cross axis running from St. Laurences' Gate to West Gate, are both in place, enclosed by the medieval wall. The quay in the area now called The Mall exists and buildings line the cross streets.

By 1657 (Newcomen's Map, Fig. 2.2), a grid layout is developing in the area around St. Laurences' Gate - a pattern which survives to a large extent today, albeit disrupted by the St. Laurence's shopping centre. The grid is formed by streets and lanes, establishing a circulation hierarchy which remains relevant and useful to the contemporary town.

There is a clarity to Ravells 1749 Map (Fig. 2.3) which also gives us an insight to the importance of Drogheda at this time, and its citizens.

The prominent formal gardens are attributed to the town's elite - Lord Chief Justice Singleton's garden, Alderman Barlow's garden, etc., - all located on the northern side, exploiting no doubt the elevated topography and fine southerly aspect to the River. Thus, at this time, a river vantage does appear to have been desirable.

Eighteenth century Drogheda is growing, urbanising and prospering. Van Hagen's (1718, Fig. 2.4) and Ricciardelli's (c. 1753, Fig. 2.5) landscape views of the town portray this urban confidence and ambition, albeit these paintings may be somewhat idealised views. However, they also suggest a careful landscape understanding of view, prospect and urban scenography which is consciously composed.

The visual aesthetic qualities of the town appear to matter as much as power and commerce at this time, perhaps it is with power and prosperity, comes confidence and ambition for the visual and aesthetic aspects of town making.

Larcom's map of 1837 (Fig. 2.6) shows the intensification of buildings within the Medieval town - expansion beyond this occurring only along the main roads leading into and out of the centre.

This map is contemporary with the first edition Ordnance Survey (Fig. 2.7), reproduced here from the Drogheda Town Walls Conservation Plan, which shows the surviving upstanding remains of the medieval walls.

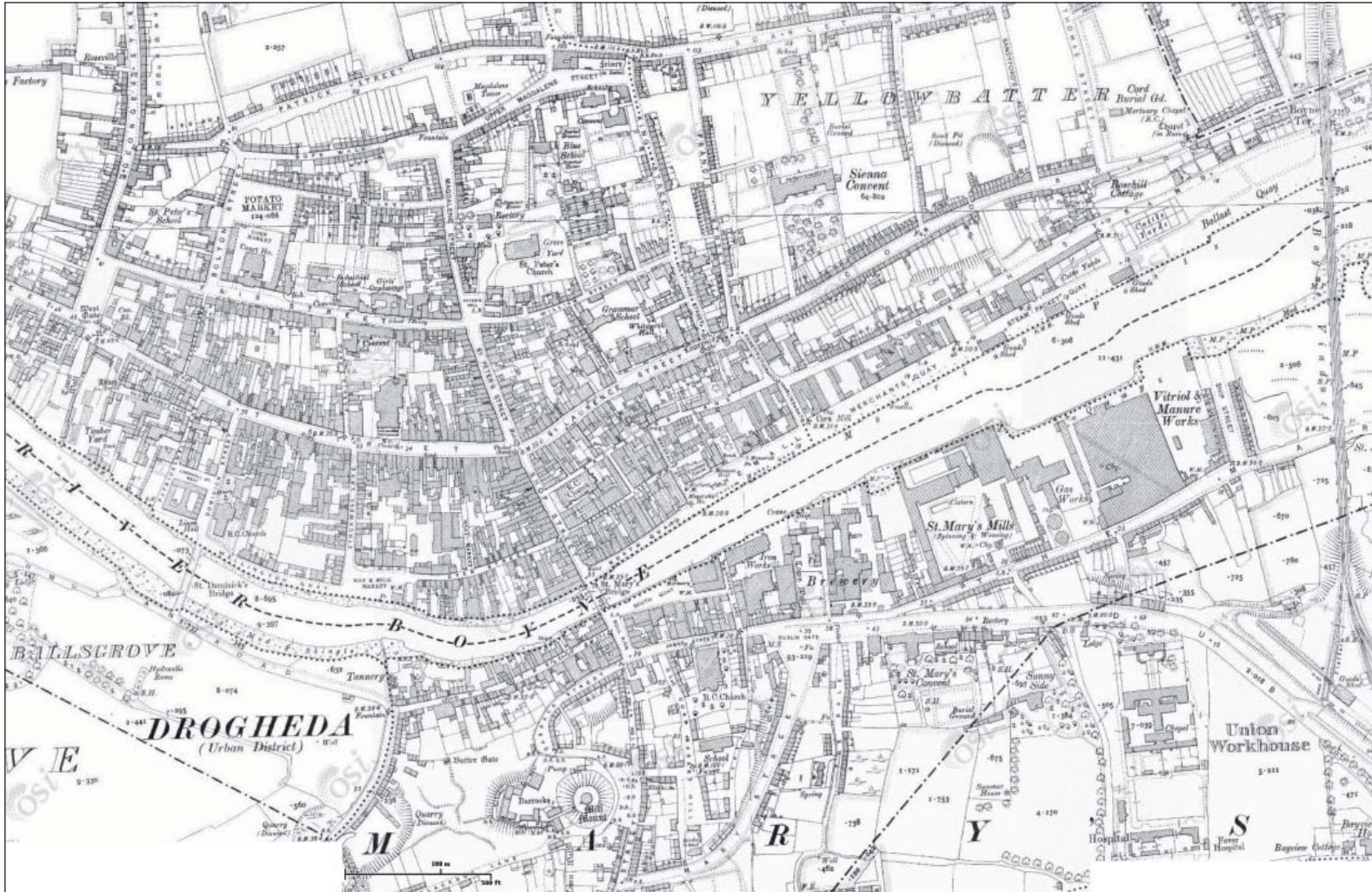


Fig. 2.8. 1911 edition, Ordnance Survey

Industrialisation, the expanded port and the number of civic, educational and religious buildings and complexes show the continued importance of Drogheda. By 1911, there are further substantial port/industrial structures along the south bank and a densification of the town centre with residential streets, courts, etc is notable. The 1911 OS map (Fig. 2.8) provides a useful baseline for the Urban Design Framework Plan, in understanding change and continuity.

The historic qualities described here have survived to a great extent into the twenty-first century, albeit with some erosion and alteration. Fig. 2.8 overlays the current building form on the 1911 OS Map with areas of notable change highlighted. On plan, the urban form reads strongly, on the ground many of the distant views retain their powerful and distinctive profile - notably from Magdalene's Tower down Peter's Hill to Millmount (Fig. 2.9); from West Street looking towards St.

Laurence's Gate (Fig. 2.10) and, east of the Boyne Viaduct looking towards the town (Fig. 2.11).

Still of high quality but less coherent are the panorama views depicted by van Hagen and Ricciardelli - the views from Ballsgrove and Millmount across the River and towards the historic town, however the essential elements are still there.



The following pages illustrate four urban spatial sequences of Drogheda which can be considered of national importance. Their value lies in the quality of spatial enclosure and the integrity and coherence of the space. These values are relevant to the contemporary urban centre and so their protection and enhancement can be understood as much as being an investment in the here, now and future of Drogheda as it can be the valuing of an historic inheritance.

These four spatial sequences are referred to again in Section 6.6, under the Public Realm strategy, which looks at these and four other components of the urban structure and form of Drogheda which should be planned, managed and designed for in an integrated and coherent way, in order to exploit the maximum potential of these spaces in creating and maintaining a quality environment for citizens and visitors alike.

Map 2.9 Overlay of 1911 OS Map on current OS Map: the black is the current built form, the grey is 1911 built form and the orange indicates buildings and street edges which no longer exist



Fig. 2.10: View from St. Peter's Hill towards Millmount



Fig. 2.11 View to St. Laurence's Gate



Fig. 2.12: View towards Drogheda from east of viaduct

The topography of Drogheda is dominant in this spatial sequence and in such a way that the river is almost a 'surprise' discovery at the bottom of both hills. The significance of the river as an administrative boundary is belied by the modest span of St. Mary's Bridge conveniently positioned at a narrowing of the Boyne

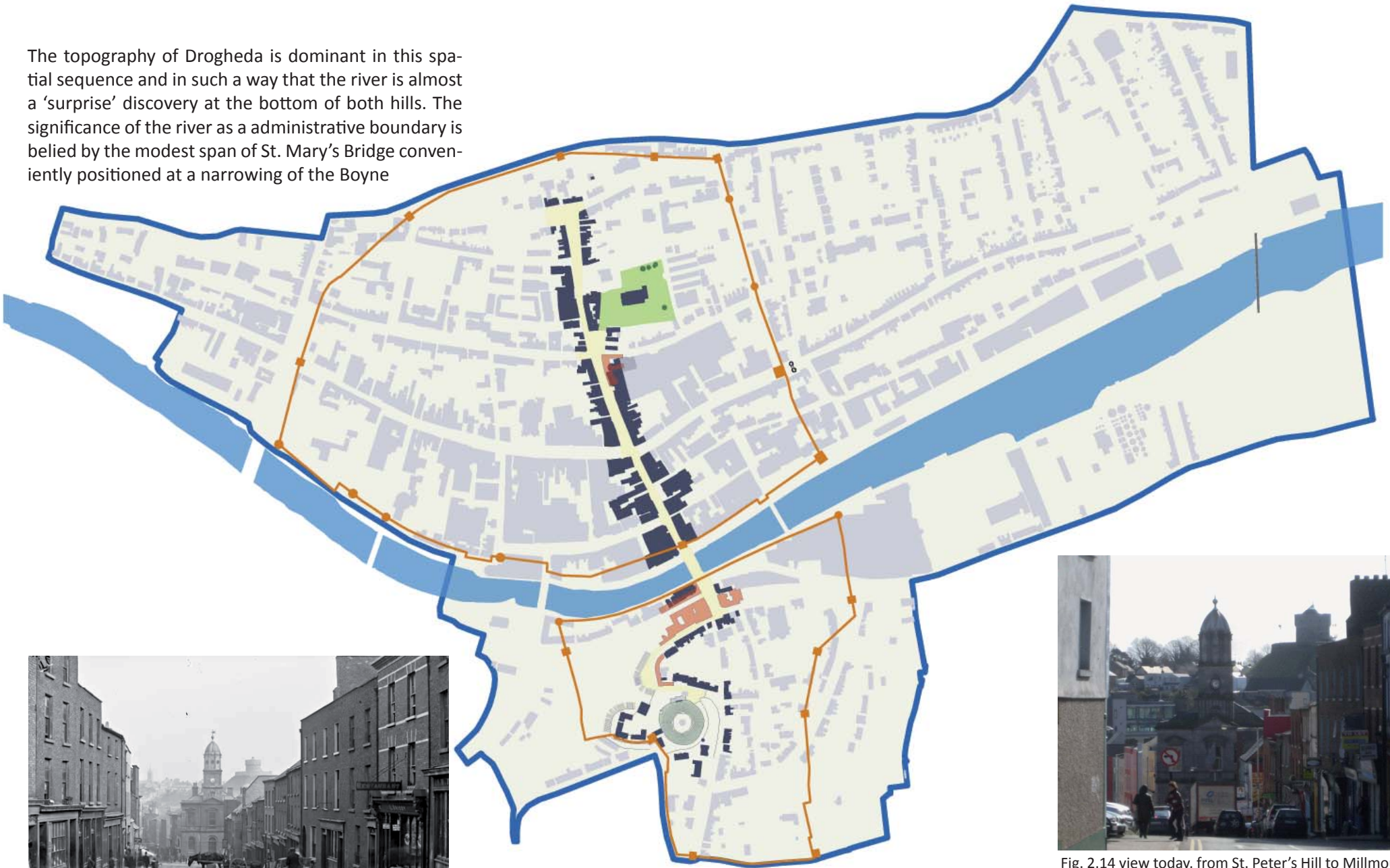


Fig. 2.13 early 20th century photograph (source: www.nli.ie)



Fig. 2.14 view today, from St. Peter's Hill to Millmount with cupola of Tholsel in mid-ground. The dominance of topography and silhouette (building form against sky/horizon) combined with historic associations of the landmark elements, contribute to memorable qualities of this spatial sequence.

Urban Spatial Sequence 1: Millmount - Bull Ring - Shop Street - St. Peter's Hill - Magdalene Street

Urban Spatial Sequence 2: West Street - St. Laurence's Street Spine

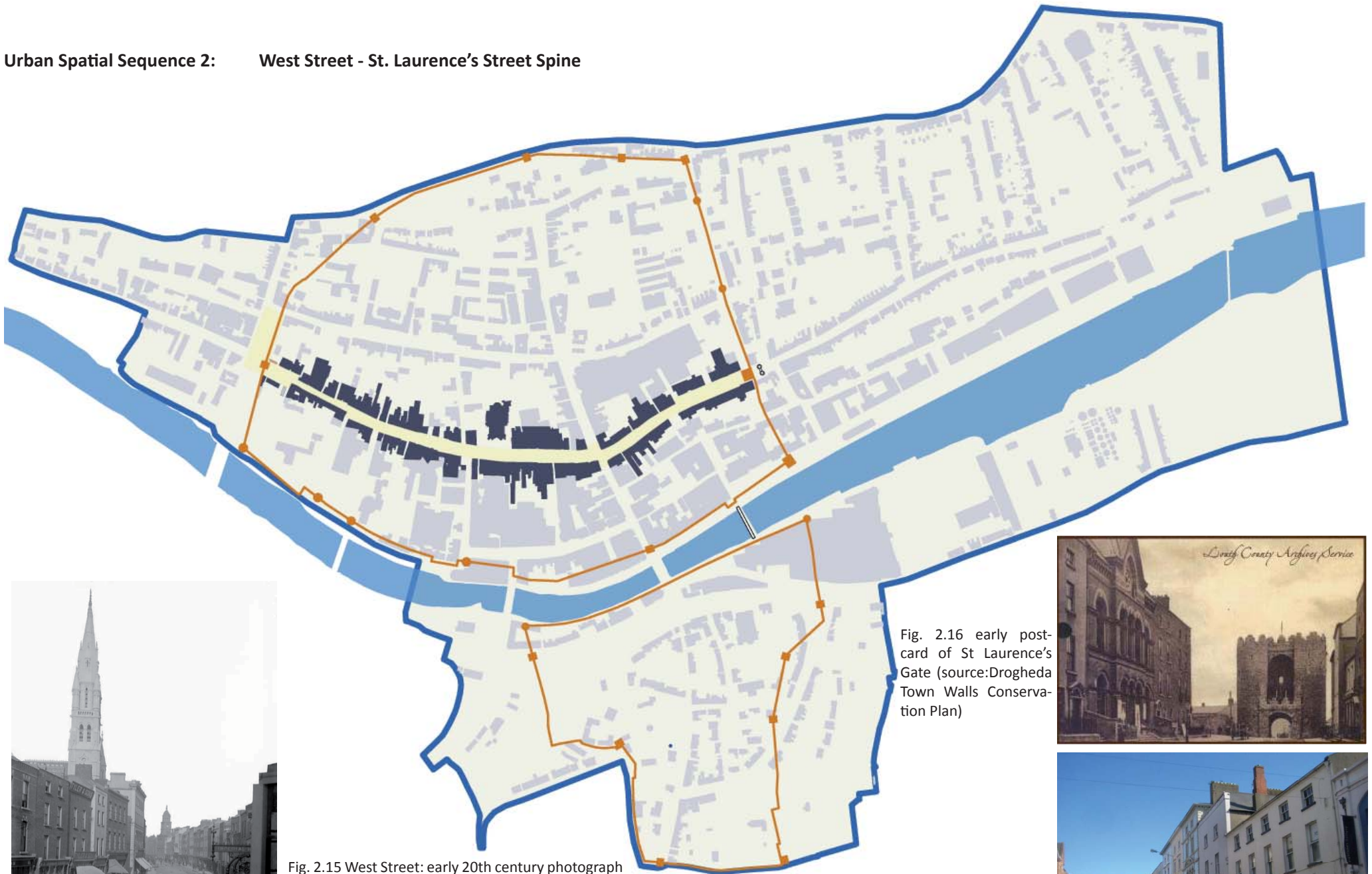


Fig. 2.15 West Street: early 20th century photograph (source: www.nli.ie). The curve, scale of buildings to street width and the vertical punctuation of spire and cupola contribute to spatial quality



Fig. 2.16 early postcard of St Laurence's Gate (source: Drogheda Town Walls Conservation Plan)



Fig. 2.17 Laurence's Street today looking towards St. Laurence's Gate. The gradual incline of street with buildings stepping in height enhance the visual impact of this medieval gate.

Urban Spatial Sequence 3: Fair Street Spine

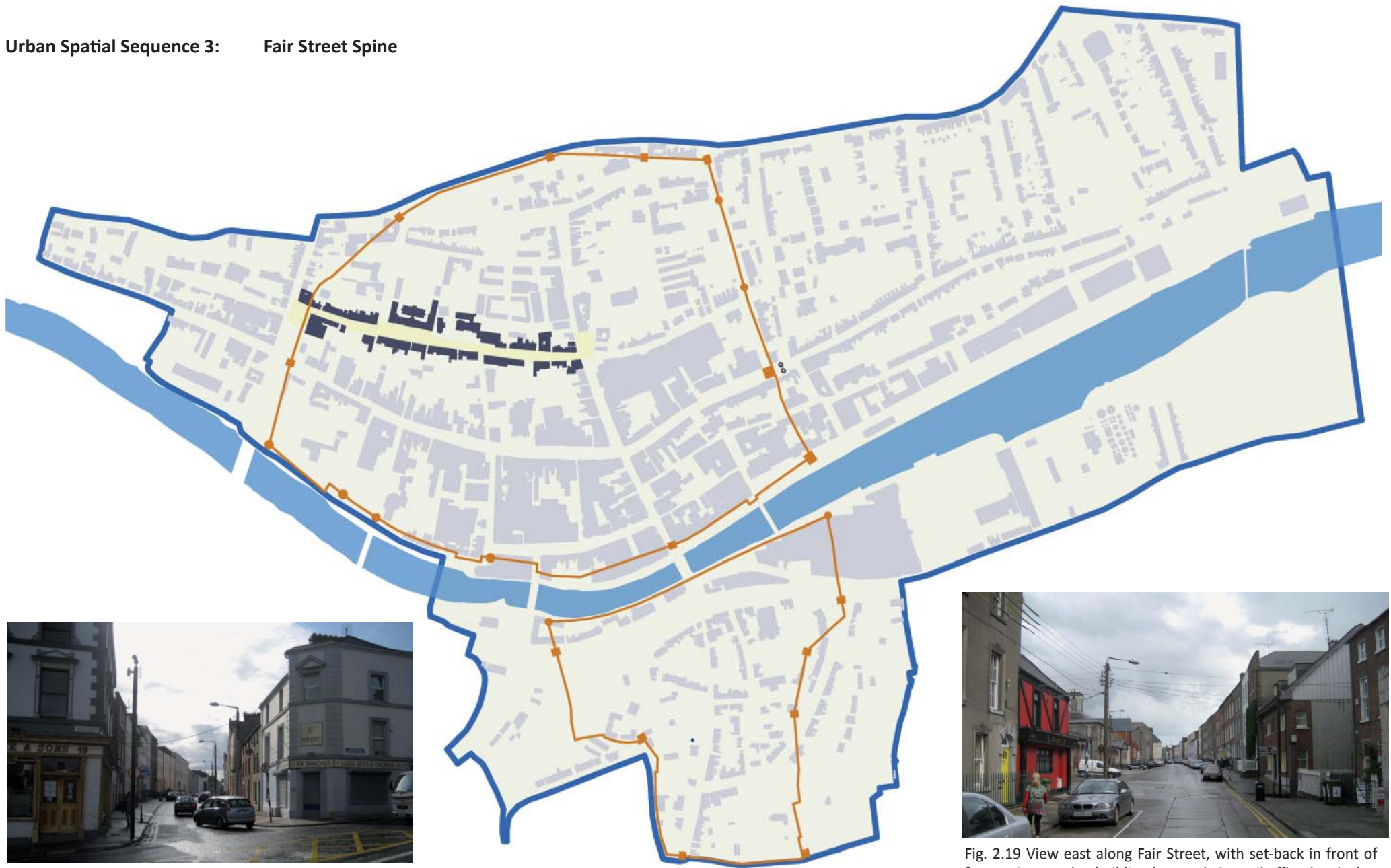


Fig. 2.18 View from St. Peter's Hill to Fair Street. Gentle sweep of street and proportion of street width to (consistent) street height create quality of spatial enclosure



Fig. 2.19 View east along Fair Street, with set-back in front of former Cornmarket building (Borough Council offices) articulating the importance of this building. The consistency of building line, height and architecture and the gentle curve of the buildings opposite create an enhance the civic qualities of the street and the Cornmarket.

Urban Spatial Sequence 4: The Mall - Merchant's Quay/ Steam Packet Quay (North Quays) / South Quay

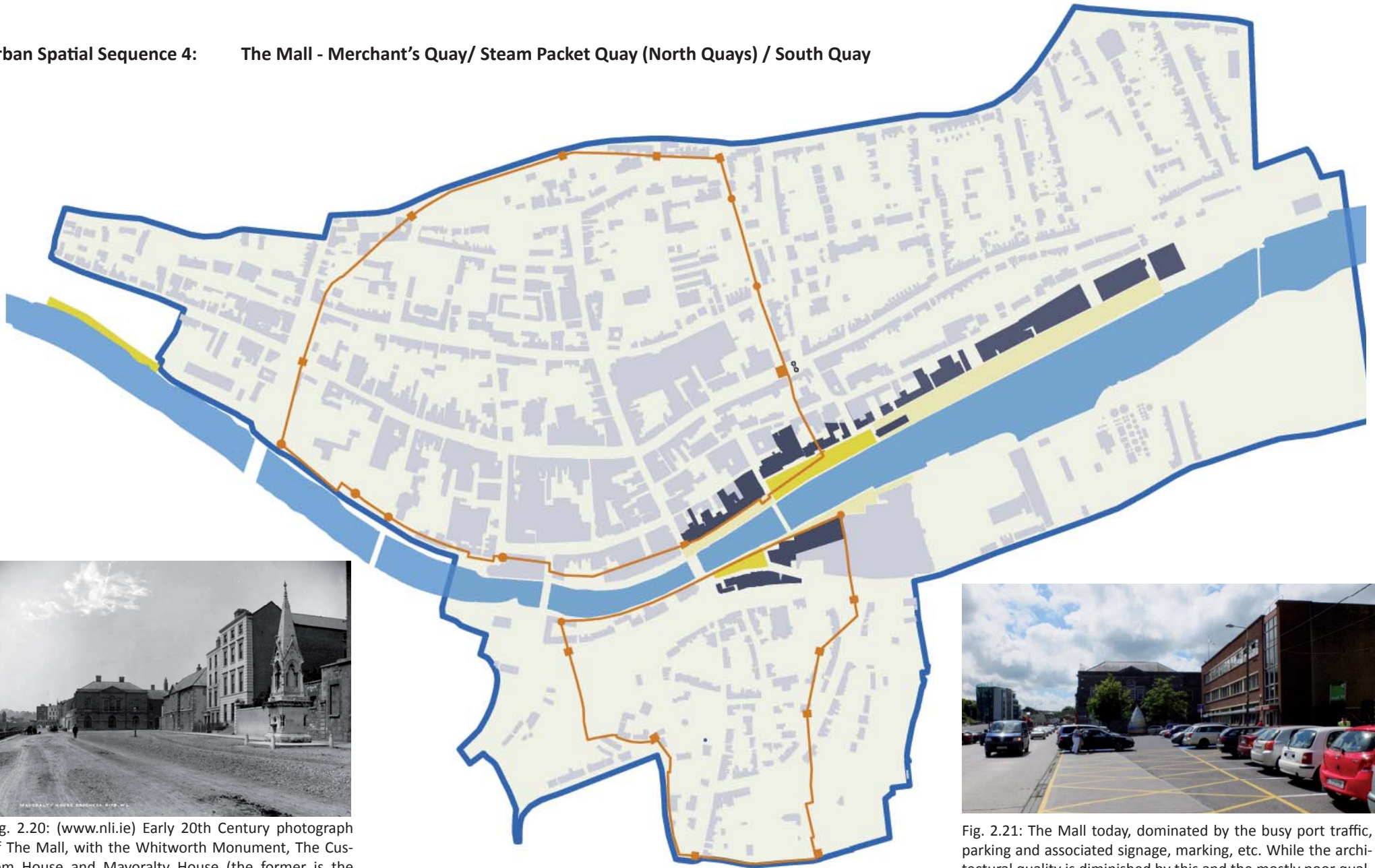


Fig. 2.20: (www.nli.ie) Early 20th Century photograph of The Mall, with the Whitworth Monument, The Custom House and Mayoralty House (the former is the sole survivor). The lack of clutter allows the spatial character to read clearly, although this is probably an early morning photograph of a normally busy place



Fig. 2.21: The Mall today, dominated by the busy port traffic, parking and associated signage, marking, etc. While the architectural quality is diminished by this and the mostly poor quality modern buildings, the spatial quality endures. The south facing river location provides a wonderful opportunity for an enhanced public realm and active uses serving culture, recreation and tourism needs and making greater use of the river.

3.0 Consultation

The following consultation was held during the Plan preparation period to inform the preparation and content of the plan.

At the outset of the plan a presentation was made to Drogheda Borough Councillors to inform the Council of the Plan methodology and focus and to invite comment and contribution.

Workshops: Two workshops were held in Drogheda.

Stakeholder (by invitation with over 190 bodies invited)
Public - open workshops with public notice/ invitation issued in advance.

In addition, the consultation meetings were held with the following organisations:

OPW
Drogheda Port
Department of Arts Heritage and the Gaeltacht (DAHG) - Architectural and Built Heritage Unit

A comprehensive report on the process and outcomes of workshop sessions is contained in Appendix A. This also includes submissions received.



Fig. 3.1: Stakeholder consultation workshop in Barlow House

4.0 Statutory Planning & Policy Framework

This section of the Drogheda Urban Design Framework Plan identifies the statutory planning context within which the Urban Design Framework for the Drogheda Heritage Area is set. Development and land use zoning aims, policies and objectives are determined by the higher ranking statutory development plans adopted by Drogheda Borough Council and Louth County Council, which have been subject to Strategic Environmental Assessment and Appropriate Assessment. These local statutory plans in turn are guided by the strategic planning policies of the Regional Planning Guidelines and National Spatial Strategy.

The Drogheda Borough Council Development Plan 2011-2017 is the most relevant reference document in relation to the implementation of strategic and local development policies and objectives at a local level within Drogheda Borough. The Urban Design Framework for the Drogheda Heritage Area can assist in achieving these statutory policies and objectives.

Identified in sections below are the relevant national, regional and local plans which provide the planning policy context for the Urban Design Framework for the Drogheda Heritage Quarter. A draft hierarchy of Plans and Programmes in which the Urban Design Framework for the Drogheda Heritage Quarter is placed in context is illustrated further below.

National Development Plan 2007 -2013

The National Development Plan (NDP) 2007-2013, sets out a detailed development strategy for the country, supported by investment in the key areas of infrastructural development, education and training, the productive sector and the promotion of social inclusion. The NDP also contains a commitment to support the National Spatial Strategy in the promotion of more balanced regional development throughout the country.

National Spatial Strategy: 2002 -2022

The National Spatial Strategy (NSS) is a twenty year planning framework designed to achieve a better balance of social, economic, and physical development between the regions and a better spatial distribution of population growth in Ireland. The strategy seeks to establish a network of development locations by identifying a hierarchy of gateways and hubs with the necessary critical mass to become self-sustaining development centres of national and regional importance.

Although the NSS identifies Drogheda as being within the Border Region, it is noted that the town's close functional and physical links with the Greater Dublin Area and its location along the Dublin-Belfast Corridor requires that the role of Drogheda be considered in relation to the GDA as well the Border Region. The NSS recognizes that:

“Drogheda has much potential for development given its scale, established enterprise base, communications and business and other links with the Greater Dublin Area.”
(NSS: 2002, p.76)

Drogheda is identified as a primary development centre, having good potential for development

given its scale, established enterprise base and links to the Greater Dublin Area, that will complement the function of the Gateways and Hubs identified under the NSS. The strategic emphasis for Drogheda at national planning level is to establish a population level that supports self-sustaining growth, but which does not undermine the promotion of critical mass in other regions.

The Council is aware of the intention of the current Minister for Environment, Mr Phil Hogan, to comprehensively review the NSS gateways and hubs strategy by 2014.

Regional Planning Guidelines for the Border Region 2010 -2022

The Regional Planning Guidelines (RPG's) for the Border Region is a long term strategic planning document which aims to direct future growth and seeks to implement the planning framework set out in the National Spatial Strategy. The vision of the RPG's is to ensure that :

“By 2022, the Border Region will be a competitive area recognized as, and prospering from, its unique interface between two economies, where economic success will benefit all, through the implementation of the balanced development model, which will provide an outstanding natural environment, innovative people, which in themselves, will be our most valuable asset” (BRA, 2010: 34)

Of relevance to the Drogheda Urban Framework Plan, a variety of environmental aims and objectives are identified for this Region, including:

- The continued protection, management and enhancement of natural heritage, built heritage and environmental resources.

- Continued development and promotion of areas of heritage value which have importance to the region, county or locality, including sourcing appropriate resources and development of management plans, for long term and sustainable investment and monitoring.
- Re-assessment of the way in which open spaces are managed, linked and developed, and the potential for integrating additional functions in that reassessment. (BRA, 2010: 118).

Regional Planning Guidelines for the Greater Dublin Area 2010 – 2022

The Dublin Regional Authority and the Mid-East Regional Authority produced the Regional Planning Guidelines for the Greater Dublin Area in 2010. The guidelines, building on the objectives of the NSS, establish a strategic planning framework for subsequent plans and future investment within the Region. The primary objective of the RPG GDA seeks to consolidate growth in the Dublin Metropolitan Area and key centres in the surrounding hinterland.

While Drogheda Borough forms part of the Border Region, due to the location of its southern environs within the GDA it has an economic and social impact on and its development is in turn strongly influenced by the GDA region. It is therefore designated as a Large Growth Town I and Primary Economic Growth Town under the RPG GDA settlement strategy.

It sits on a multi-modal transport corridor and is thus considered a key location for services, retail and economic activity (including tourism) to support its urban and rural hinterland population. Building upon its historic and heritage context,

the development of tourism in Drogheda is seen as part of the branding exercise for the area, under the RPG GDA.

Balanced against the promotion of its economic development, environmental objectives similar to those included in the Border RPGs, recognise the need to protect, manage and enhance natural heritage, built heritage and environmental resources in a plan led manner.

Retail Planning Guidelines 2012

The Retail Planning Guidelines 2005 were updated in 2012. The amended Guidelines include recommendations in relation to retailing and design quality (Section 5). They identify that where heritage issues are likely to be a major consideration in the development of a town, because of the wealth of protected structures, architectural conservation areas (ACAs) and archaeological features, an urban framework approach is recommended (either statutory LAP or non-statutory planning framework). Within this planning context, issues such as traffic congestion and car parking considerations arising from historic street pattern, accommodating appropriate floor plate sizes and retail formats and linkages to different shopping areas, etc.

DAHG Guidelines on the Protection of Architectural Heritage for Planning Authorities, 2011 (protected structures and ACA's)

Design Manual for Urban Roads and Streets (March 2013).

Recently published, this makes a significant development in approaches to urban street making, promoting an integrated urban design approach.

Louth County Development Plan 2009 – 2015

The Louth County Development Plan provides an overall development strategy for the sustainable development of County Louth for the period 2009-2015. The vision of the plan is to ensure that Louth is a place:

- Where people want to live, work, visit and invest now and in the future
- That is well planned, well managed, safe and inclusive
- Where there exists equality of opportunity for all

Under the Louth County Development Plan, Drogheda is designated as a top tier settlement (Level 1) in its settlement hierarchy. The Plan supports the policies and objectives of Drogheda Borough Council to develop Drogheda as a self-sustaining settlement providing employment opportunities and a vital and viable town centre for its inhabitants and delivering economic benefits to the surrounding hinterland, in accordance with its strategic function as identified in the NSS and RPGs noted above. The County Development Plan generally seeks to preserve, enhance and protect the quality of the environment, whilst facilitating and encouraging development.

Although setting the strategic planning policy framework for the county, the Louth County Development Plan area excludes the municipal boundaries of Drogheda Borough Council. Therefore, it is the statutory development policies, objectives and standards of the Drogheda Borough Development Plan 2011-2017 that are the most locally relevant in the context of promoting development within the Drogheda Urban Framework Plan area.

Drogheda Borough Council Development Plan 2011 – 2017

The Drogheda Borough Council Development Plan 2011–2017 provides the most directly relevant, primary statutory planning policy framework for the sustainable development and management of land uses within the Council’s jurisdiction, including the Urban Framework Plan area.

The economic, social and cultural development policies and objectives of the statutory Development Plan have had regard to all of the aforementioned national, regional and local strategic planning policies of relevance to Drogheda.

Under the Borough Development Plan the context of Drogheda’s Town Centre and wider Heritage Quarter is clearly identified. The Development Plan establishes a number of important policies which are intended to direct future development into the Town Centre and Heritage Quarter. To provide additional guidance, it is a stated policy of the Development Plan that, an Urban Design Framework Plan for the Heritage Quarter is to be prepared.

In the context of the heritage context of the town, the Development Plan seeks to:

- Protect and enhance the natural and built environment
- Safeguard the natural and built environment as a resource in its own right.
- Interpret and promote the importance of archaeological, architectural and other aspects of the Built and Natural environment of cultural significance. (DBC, 2011: 158)

Drogheda Port LAP 2007-2012:

The Drogheda Port LAP includes lands which also lie within the Heritage Quarter area. The LAP proposed extensive mixed use redevelopment of these lands in particular, envisaging the gradual move of traditional port activities away from the historic centre/port area, eastwards. Deeper berth facilities have been created here, however, Drogheda Port Company has reviewed this strategy and envisage continuing port activity on the North Quays for the future.

Other Plans of Relevance

There is a broad range of other statutory and non-statutory plans which have been considered by the Council in devising the Drogheda Urban Framework Plan. A broad hierarchy of plans (not exhaustive) is identified below, with the statutory plans of particular relevance to development within the local area highlighted in bold text.

International Level (non-satutory):

- Leipzig Charter on Sustainable European Cities (2007) - seeks integrated planning, improved public spaces, infrastructure and energy efficiency; recognises importance of distinctive character of cities and towns
- Charter for the Conservation of Historic Towns and Urban Areas (Washington Charter 1987)
- UNESCO Recommendations on the conservation of the Historic urban landscape (2011) - a voluntary charter. This takes a landscape approach recognising the “*dynamic nature of living cities*” and places with emphasis on the integration of urban heritage conservation within general policy planning and practices The historic urban landscape is defined as “*the urban area understood as the result of a historic layering of cultural and natural values and attributes...*” It thus extends beyond the “monu-

ment” and its setting (primarily understood as its physical setting), to embrace a wider context which includes “... *the site’s topography, geomorphology, hydrology and natural features; its built environment, both historic and contemporary; its infrastructures above and below ground; its open spaces and gardens, its land use patterns and spatial organisation; perceptions and visual relationships; as well as all other elements of the urban structure. It also includes social and cultural practices and values, economic processes and the intangible dimensions of heritage as related to diversity and identity.*”

This landscape approach is grounded in the local and aimed at preserving the quality of the human environment, allowing for cultural diversity and creativity and ensuring that “contemporary interventions are harmoniously integrated with heritage in a historic setting...”

- EU Directive on Strategic Environmental Assessment (2002)

National Level

Water Framework Directive 2012; Appropriate Assessment of Plans & Projects in Ireland (NPWS 2009); Sustainable Residential Development in Urban Areas 2009 & associated Urban Design Manual – A Best Practice Guide (see below); Flood Risk Guidelines for Local Authorities 2009; Energy Policy Framework 2007-2020; Quality Housing for Sustainable Communities 2007; National Development Plan 2007-2013; National Climate Change Strategy 2007; National Heritage Plan 2002; National Inventory of Architectural Heritage Act, 1999; National Biodiversity Plan 2002; Shaping the Future: Re-Utilisation Best Practice Guide, (DAHG, 2012; Development Contribution Scheme for Local Authorities (2012).

- Urban Design Manual - A Best Practice Guide (2009): The Urban Design Manual is a non-statutory design manual that introduces the key principles of urban design and sustainability insofar as they relate to residential development, particularly within the 30-50 dwellings per hectare density range. The guide is based around 12 criteria that reflect the sequence of a good design process, working from the neighbourhood level, down to the site level, and finally to the individual home level. Although targeted at medium density residential development, all of the 12 criteria such as 'Context; 'Public Realm', 'Adaptability, and 'Distinctiveness' set down core placemaking principles

- Conservation Plan for Town Walls and Other Defences of Drogheda, 2006: Provides policies and strategies for conservation and presentation of walls, both where they survive in an upstanding condition and where above ground fabric no longer survived. The approach is to integrate the medieval walls within any development, including public realm works in accordance with defined conservation and urban design strategies. In this way the medieval walls can be recovered as a key component of Drogheda's urban structure (ref section 6.4 where this is further elaborated).

The Drogheda Active Travel Town Walking and Cycling Strategy 2013 was in development during the preparation of the Urban Framework Plan and was considered within the plan making process.

Regional Level

Regional Planning Guidelines for the Border Region 2010-2022; Regional Planning Guidelines for the Greater Dublin Area 2002-2020; Eastern River Basin District Management Plan 2009-2015; Planning Strategy for the Greater Drogheda Area 2008-2024; Flood Risk Management Plans; Groundwater Protection Schemes; North East Regional Waste Management Plan 2005-2010.

Local Level

Drogheda Southern Environs Local Area Plan 2009-2015; North Drogheda Environs Local Area Plan; Economic Strategy for Drogheda and its Environs 2009-2015; M1 Economic Corridor Development Study 200-2022; Planning Strategy for Greater Drogheda Area; Drogheda Transportation Study 2006; Drogheda Biodiversity Action Plan; Louth Heritage Plan 2007-2011; County Louth Biodiversity Action Plan ;2008-2012 Drogheda Biodiversity Action Plan 2009; A Guide to Architectural Conservation Area in Louth; Guidelines for Works in Architectural Conservation Areas in Louth.

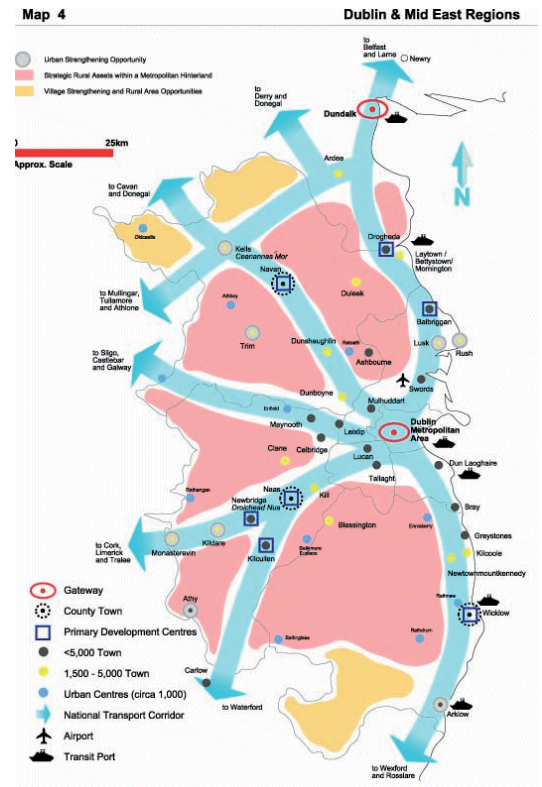


Fig. 4.1: Extract from National Spatial Strategy: Dublin & Mid-East Region

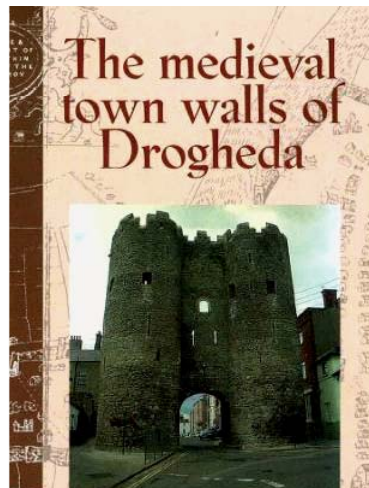


Fig. 4.2: Cover of Conservation Plan for Medieval walls

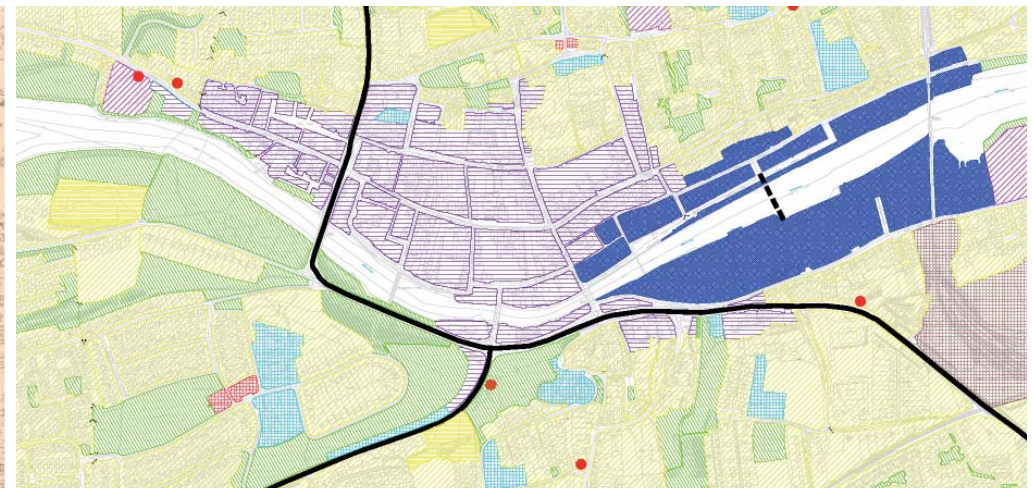
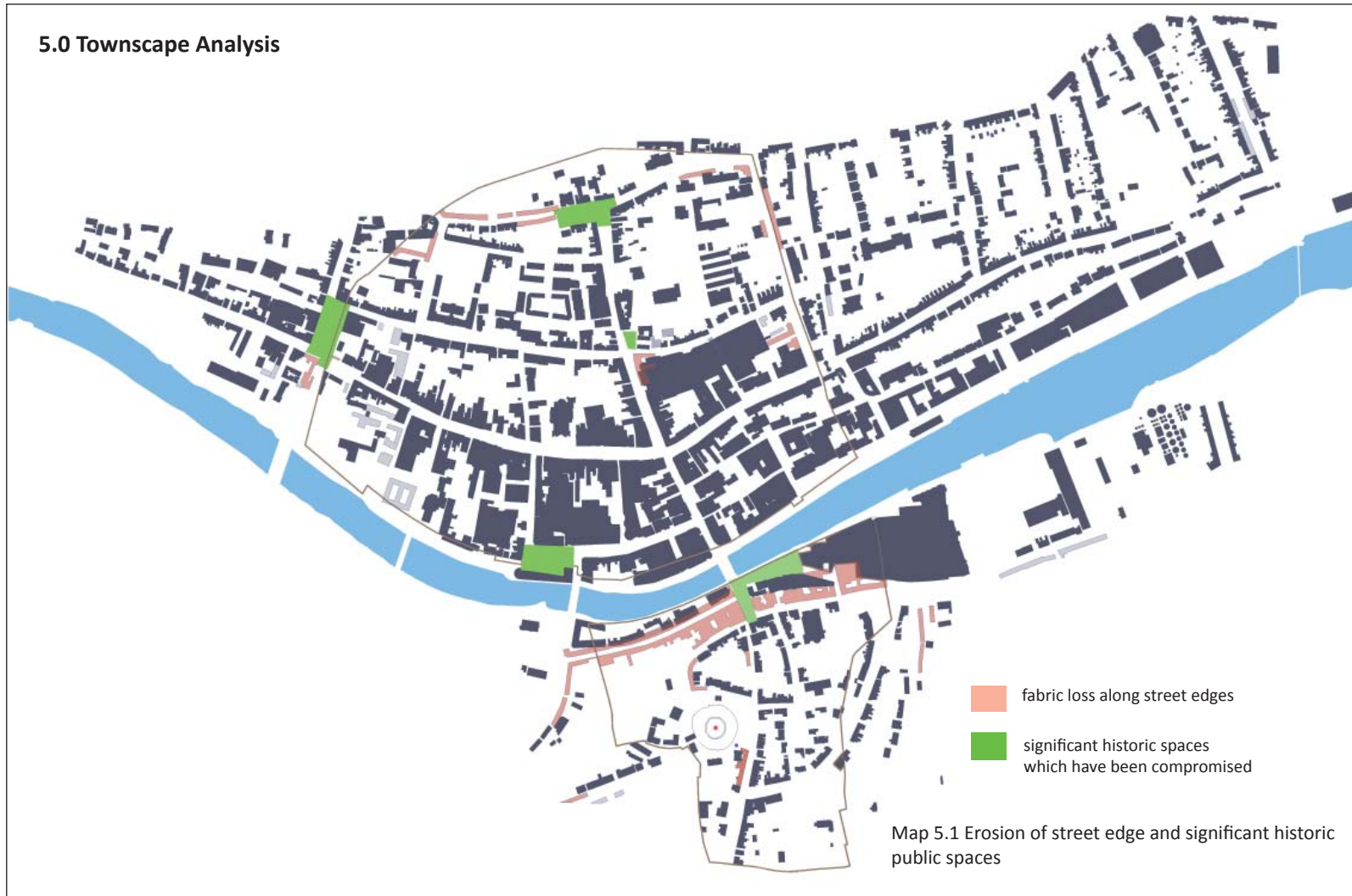


Fig. 4.3: Land Use Zoning Objectives Map (Drogheda Borough Development Plan 2011-2017)

5.0 Townscape Analysis



Map 5.1 left shows a number of changes to Drogheda's urban form over the last 100 years. Firstly, the orange indicates areas where street edge has been eroded through loss of built fabric, alterations to roads, streets, etc. This is particularly notable where the Dublin Road meets the Bull Ring/South Quay node, a significant gateway to the town centre where vehicles are prioritised over pedestrians and cyclists.

Secondly, a number of public spaces of historic and spatial importance have been compromised through fabric decay, under/low value use and public realm presentation. Mostly these spaces can be recovered and improved, in one instance the historic space - Haymarket - has been built over, the loss here is likely to be long-term.

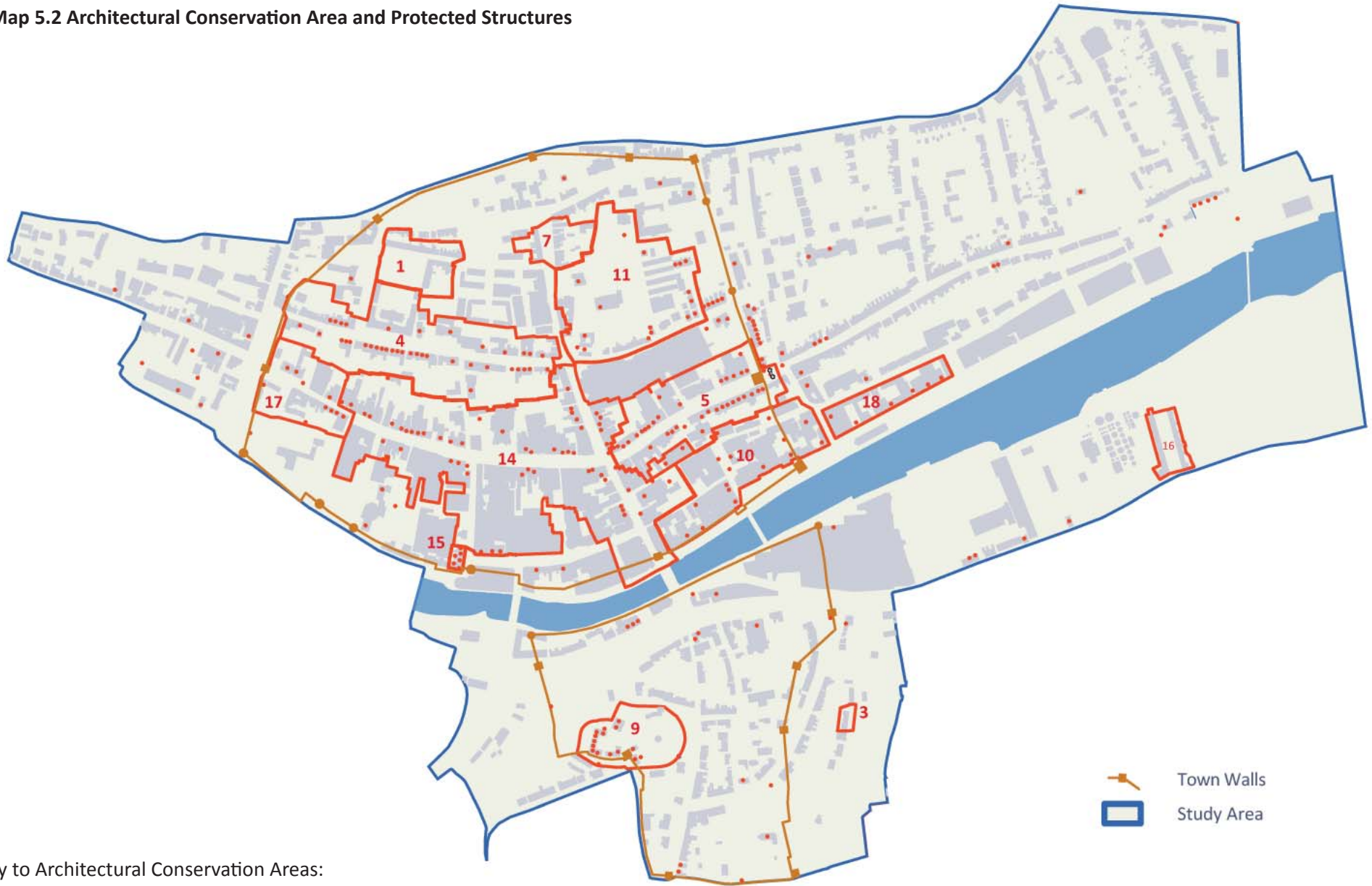
Thirdly, the long narrow plot subdivision within the larger urban block has, in a number of locations, been filled in with large footprint blocks, in places removing an historic laneway from public use and thus reducing permeability.

Notwithstanding the above changes, Drogheda's urban form still remains strong as noted previously in Chapter 2.

Chapter 2 provided a description of the principal defining characteristics of Drogheda's historic urban character. This section provides further qualitative analysis on a number of other components which contribute to the prevailing townscape of Drogheda today. Townscape can be defined as urban form and its visual appearance; the appearance of streets, including the way the components of a street combine in a way that is distinctive to a particular locality.

These components are the Architectural Conservation Areas and Protected Structures (Map 5.2), which comprise the areas and buildings deemed to be of significant architectural heritage and thereby statutorily protected; the public realm - principal paved (hard) spaces, principal green spaces and the main riverside spaces (Maps 5.3 to 5.7); Views and Vistas (Map 5.8), the areas given over to public parking within the Heritage Quarter (Map 5.9) and, a composite townscape condition assessment (Map 5.10).

Map 5.2 Architectural Conservation Area and Protected Structures



 Town Walls
 Study Area

Key to Architectural Conservation Areas:

- 1: Bolton Street, including Bolton Sq. & part of Green Lanes
- 3: The Dale - Dalebrook Cottages
- 4: Fair Street including Borough Offices
- 5: Laurence's Street
- 7: Magdalene Street (North)

- 9: Millmount
- 10: North Quay, including Back Lanes
- 11: St. Peter's Church of Ireland & Environs
- 14: West Street & Surrounding Streets Forming Drogheda's Shopping Centre

- 15: Leyland Place
- 16: Ship Street
- 17: Old Abbey Lane
- 18: Merchant's Quay

Public Space Analysis:

Public Open Spaces are considered under three types - (Paved) Urban Spaces (Map 5.3); Green Open Spaces (Map 5.4) and, River Corridor Spaces (Map 5.7). An Analysis Matrix is provided for each of these category of spaces and this is divided into 5 sections:

- Events
- Ambience
- Activity
- Movement
- Nature

The presence or absence of elements or themes within these categories, and whether there is potential for change or improvement is cross referenced to the spaces. Reading the matrix according to each space, it is apparent whether it is underused, or has potential. Likewise, each theme can be read across the matrix, identifying if there is a common issue across spaces in Drogheda.

The spaces are also referred to as Links or Places. (see Link and Place section)

Events: This section expresses whether the space is subject to Public Events, or if it has potential for this. This could be on a once-off or regular basis. Market/Stalls are specifically mentioned due to the retail character of the Heritage Quarter.

Conclusions:

- Some key spaces could be used more effectively for Public Events.
- No specific space is identified as the Main Event Space in the town.

Ambience: This aims at analysing whether the spaces have existing, innate or potential Cultural Heritage value. Qualities are expressed as Poor or Good. The presence of elements which assist in the expression of cultural heritage are expressed as yes or no. Views and Microclimate are important factors in the attractiveness and comfort of the space. A feature, such as a sculpture indicates a focus for the space, or may be the presence of a bridge or tower or other artefact. The use of the space at Night time typically indicates that it can extend its uses for periods into the evening. This is particularly important for spaces associated with restaurants, bars, theatre and cinema. Information indicates whether the heritage value of a space has been interpreted for a visitor.

Conclusions:

- Many spaces with innate heritage value.
- Many spaces with good views and potentially attractive settings.
- Many spaces with good microclimate, or potential for mitigating poor microclimate.
- Heritage information is not always apparent, apart from at key locations.

Activity: This section generally indicates whether a space is achieving full potential in its use and engagement by the public. An Interactive Point indicates elements such as a telephone box, free wifi, or a kiosk etc. Recreation and Play indicate whether there are opportunities for play for all ages, as well as whether there actually is a playground or equipment.* Seating and quality of Street Furniture are good indicators of the amenity value of a space, and usually help with making comfortable and safe places to 'hang out' (informal), or socialise (formal). Passive supervision of a space is reliant on the latter, and on the urban form of the edges of the space.

Conclusions:

- There are a lot of spaces that do not realise their full potential to become active and useful places within the Heritage Quarter.

Movement: This section deals with the ability to arrive at and get into a space. Public Transport and Drop Off areas also indicate that a space may be activated by a bus stop or taxi rank, for instance. Although the topography in the heritage quarter can be steep, the ability to Cycle to a space and to leave a bike at a Cycle Rack will encourage people to cycle from the surrounding areas. The topography also affects Pedestrian Accessibility, and there are ways of mitigating this, but more often refers to wayfinding and quality of footpaths and crossings.

Conclusions:

- Accessibility and Wayfinding is an issue across many spaces.

Nature: The presence or absence of Biodiversity** is an indicator of the natural quality and value of a space. Lawns and Planting (e.g. flowering shrubs, planters, window boxes) generally indicates the civic status of a space, and it is important to note that Trees and quality of urban forestry are strong indicators of civic amenity.

Conclusions:

- There are not always obvious or appropriate ways of introducing biodiversity into urban spaces
- Improved biodiversity can be implemented or managed
- There is scope for more tree planting in appropriate spaces.

*refer also to Louth Local Authorities Sports and Recreation Strategy 2006-2012, Louth Play Plan (Draft 2005) and Meath Play Plan.

**refer also to Biodiversity Action Plan for Louth 2008-2012 and Drogheda Biodiversity Action Plan, 2009.

Map 5.3 Urban (paved) Spaces



- Public Space
- Town Walls
- Study Area

- 1: Millmount (external)
- 2: Millmount (internal)
- 3: Dublin Gate
- 4: Bull Ring
- 5: South Quay
- 6: Donor Road Junction
- 7: Linen Hall (car park)

- 8: Haymarket
- 9: St. Dominick's/Shop Street
- 10: The Mall
- 11: St Laurence's Gate
- 12: West St/Tholsel
- 13: West St. (St. Peter's Piazza)
- 14 West Gate

- 15: Fairgreen
- 16: Potato Market
- 17: Cornmarket
- 18: St. Peter's Hill
- 19: Sunday's Gate
- 20: Magdalene St/Rope Walk
- 21: St. Mark's Terrace, Scarlet Street

- 21: Bredin Street jct.
- 22: King Street car park
- 23: Bolton Gate
- 24 Prospect Avenue



Fig. 5.1 St. Peter's Hill -space characterised by widening out to mark entrance to St. Peter's; slope; southerly aspect and view to Millmount



Fig. 5.3 Millmount: The former military buildings form public spaces of distinction, somewhat compromised by car parking uses but these can be 'recovered' for temporary uses - festivals, markets, etc.

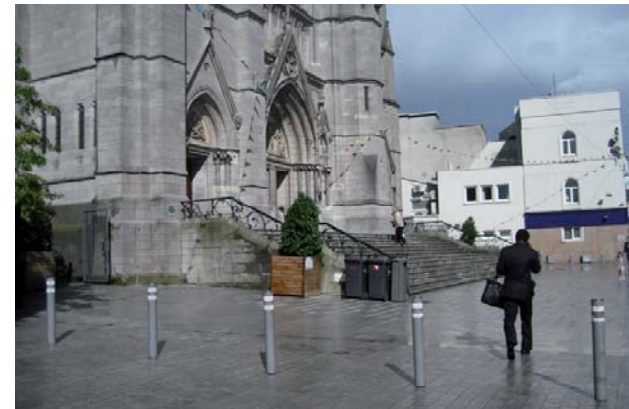


Fig. 5.14: West Street/St. Peter's Piazza: The recent public realm works at West Street provide an improved setting for St. Peter's, acknowledging its civic qualities. This could be extended into the street/carrageway by omitting parking and creating a stronger focal point.



Fig. 5.2: Forecourt to Cornmarket. This space relates both to the Potato Market through associated functions with former market building (now local authority offices) and, to Fair Street which it forms part of. Future development of Drogheda Borough Council offices provide an opportunity to formally acknowledge this relationship and enhance this space and the Potato Market.

These urban (paved) spaces vary considerably in type and character.

- The 'Gates' - Dublin, West, St. Laurence's, Sunday's and Bolton - relate to the medieval wall enclosure, marking historic gateways into the medieval town.
- Others relate to historic civic functions - Bull Ring, South Quay, Linen Hall, Haymarket, Fairgreen, Potato Market/Cornmarket.
- The Mall, West St (St. Peter's Piazza), St. Dominick's/Shop Street, St. Peter's Hill and St. Mark's Terrace/Scarlet St. are settings to important buildings.
- Nodal spaces occur at the junction of primary streets, distinguished by distinctive spatial enclosure - West St/Tholsel; Magdalene St/Rope Walk. . and, of lesser urban presence, Donor Road junction, Bredin Street jct and Prospect Avenue.
- The Millmount spaces are both setting and functional, giving definition to this historic complex.
- Finally, King Street car park might be considered quasi-public space, ultimately the reinstatement of a built edge here may be desirable.

The following tables analyse these spaces under a range of criteria which are reflected in recommendations contained in Section 6.6



Fig. 5. 15. West Gate: A section of upstanding medieval wall survives behind a temporary hoarding and the spatial character of this place reflects its historic gateway origins. However, the potential of this space is compromised by its surface car park use, fragmented built edge and dominance of passing traffic.

Place / Activity URBAN SPACES	1.MillMount external	2.Millmount internal	3.Dublin Gate	4.Bull Ring	5.South Quay	6.Donore Road jct	7.Linen Hall (car park)	8.Haymarket/ Leyland Place	9.Shop St (Church)	10.The Mall	11.St Laurence's Gate	12.West St (Thosel)	13.West St (Church)	14.West Gate/George's Square	Place / Activity
LINK/PLACE	Place	Place	Place	Link	Place	Link	Place	Link	Place	Place	Place	Link	Place	Place	LINK/PLACE
EVENTS															EVENTS
Public Event	No	Yes	No	No	No	No	No	No	No	Yes	Yes	No	Yes	No	Public Event
Market / stalls	No	No	No	No	No	No	No	No	No	No	No	Yes	No	No	Market / stalls
AMBIENCE															AMBIENCE
Cultural Heritage/Setting Views	Poor	Yes	Poor	Poor	Yes	Poor	Poor	Good	Good	Good	Good	Yes	Good	Poor	Cultural Heritage/Setting Views
Microclimate	Good	good	good	poor	good	good	poor	Good	Good	Good	Good	Good	Good	Good	Microclimate
Feature/ Sculpture Nighttime	No	Yes	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	No	No	No	Feature/ Sculpture Nighttime
Information	No	yes	No	Yes	Yes	No	No	Yes	No	No	Yes	Yes	No	No	Information
ACTIVITY															ACTIVITY
Interactive point	No	No	No	No	No	No	No	No	No	No	No	No	No	No	Interactive point
Recreation/Play	No	No	No	No	No	No	No	No	No	No	No	No	No	No	Recreation/Play
Seating	Poor	No	No	Yes	Yes	No	No	Yes	No	No	Yes	Yes	Yes	No	Seating
Street Furniture	Poor	Poor	Poor	Poor	Good	Poor	Poor	Yes	Poor	Poor	Good	Good	Good	Poor	Street Furniture
Hangout	No	No	No	No	No	No	No	Yes	Yes	No	No	No	Yes	No	Hangout
Social	No	Yes	No	No	Yes	No	No	Yes	Yes	No	No	No	Yes	No	Social
MOVEMENT															MOVEMENT
Public transport	No	No	No	Yes	No	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Public transport
Drop-off Point	No	Yes	No	No	No	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Drop-off Point
Cycle racks	No	No	No	No	No	No	No	Yes	No	No	No	Yes	Yes	No	Cycle racks
Cycle	No	No	No	No	No	No	No	No	No	No	No	Yes	Yes	No	Cycle
Pedestrian accessibility	Poor	Poor	Yes	Poor	Yes	Poor	Poor	Yes	Poor	Poor	Yes	Yes	Yes	Poor	Pedestrian accessibility
NATURE															NATURE
Biodiversity	Poor	Good	Poor	Poor	Poor	Good	Good	No	Poor	Poor	Poor	Poor	Poor	Poor	Biodiversity
Lawn	Yes	No	No	No	No	Yes	No	No	No	No	No	No	No	No	Lawn
Planting	Yes	Yes	No	No	No	Yes	Yes	No	No	No	Yes	Yes	Yes	No	Planting
Trees	Yes	No	No	No	No	Yes	Yes	Yes	No	No	No	No	Yes	No	Trees

Shaded categories indicate potential for change or improvement.

Table 5.1: Key Urban (Paved) Spaces Analysis (sheet 1)

Place / Activity URBAN SPACES	15.Fairgreen	16.Potato Market	17.Cornmarket	18.Peter's Hill Sq	19.Sunday's Gate	20.Magdalene St/Rope walk	21.St Mark's Terrace/Scarlet St.	21.Bredin St Jct	23.King St Car Park	24.Bolton Gate	25.Prospect Avenue	26. Ship Street	Place / Activity
LINK/PLACE	Link	Place	Place	Place	Link	Link	Link	Link	Place	Place	Link	Place	LINK/PLACE
EVENTS													EVENTS
Public Event	No	Yes	Yes	No	No	No	No	No	No	No	No	Yes	Public Event
Market / stalls	No	No	No	No	No	No	No	No	No	No	No	No	Market / stalls
AMBIENCE													AMBIENCE
Cultural Heritage/Setting	Poor	Poor	Good	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Cultural Heritage/Setting
Views	Poor	Poor	Yes	Good	Poor	Good	Poor	Poor	Poor	Good	Poor	Good	Views
Microclimate	Good	Good	Good	Good	Poor	Poor	Poor	Poor	Good	Poor	Poor	Good	Microclimate
Feature / Sculpture	No	No	No	No	No	No	No	No	No	No	No	No	Feature / Sculpture
Nighttime	No	No	No	No	No	No	No	No	No	No	No	No	Nighttime
Information	No	No	No	Yes	No	No	No	No	No	No	No	No	Information
ACTIVITY													ACTIVITY
Interactive point	No	No	Yes	No	No	No	No	No	No	No	No	No	Interactive point
Recreation/Play	No	No	No	No	No	No	No	No	No	No	No	No	Recreation/Play
Seating	No	No	No	No	No	No	No	No	No	No	No	No	Seating
Street Furniture	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	No	Street Furniture
Hangout	No	No	No	No	No	No	No	No	No	No	No	No	Hangout
Social	No	No	Yes	No	No	No	No	No	No	No	No	Yes	Social
MOVEMENT													MOVEMENT
Public transport	No	Yes	No	No	Yes	Yes	Yes	Yes	No	No	No	No	Public transport
Drop-off Point	Yes	Yes	Yes	yes	No	No	No	No	Yes	No	No	Yes	Drop-off Point
Cycle racks	No	No	No	No	No	No	No	No	No	No	No	No	Cycle racks
Cycle	No	No	No	No	No	No	No	No	No	No	No	No	Cycle
Pedestrian Accessibility	Poor	No	Yes	poor	Poor	Poor	Poor	Poor	Poor	No	Poor	Poor	Pedestrian Accessibility
NATURE													NATURE
Biodiversity	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Good	Poor	Biodiversity
Lawn	No	No	No	no	No	yes	No	No	No	Yes	No	No	Lawn
Planting	No	No	Yes	no	No	yes	No	No	No	No	No	Yes	Planting
Trees	No	No	No	no	No	yes	No	No	No	No	No	No	Trees

Shaded categories indicate potential for change or improvement

Table 5.2: Key Urban (Paved) Spaces Analysis (sheet 2)

Map 5.4 Green Open Spaces



- 1: St. Mary's Cemetery
- 2: The Dale
- 3: Buttergate
- 4: Marsh at Boyne Viaduct
- 5: North Strand Esplanade / Doner's Green

- 6: Cord burial ground
- 7: Sienna Convent and cemetery
- 8: St. Peter's Graveyard
- 9: Magdalene St/Rope Walk green space
- 10: Bolton Gate green space

- 11: Marion Close
- 12: Front Gardens (not shown on this map)
- 13: Private Gardens (not shown on this map)

The Heritage Quarter contains a number of 'green' spaces of varying character and quality. The extent and nature of public access varies - not all are in public ownership and the church grounds, in particular make up a significant area of the green spaces to which public access is possible, albeit these are private grounds. Other green spaces, while publicly owned, lack definition and appear underused.

The green spaces within Drogheda are extremely important in the context of future development. These need to be considered in terms of the following:

- potential biodiversity enhancement and linkages as part of a Green Infrastructure network
- their role as amenity spaces for adjacent residential developments - part of neighbourhood making
- their role in making Drogheda a pleasant place at all stages of life - children to elderly

Section 6.4 sets out recommendations for the green spaces of Drogheda which reflect the analysis set out in Table 5.3.

Maps 5.5 and 5.6 address the provision and quality of play space in Drogheda. This is an important consideration for the town's aspiration to remain and improve as a place to live. Without a sufficient number and diversity of such spaces, well distributed and maintained, Drogheda will struggle to attract new residents with a commitment to settle and raise families within the town. If these challenges can be met, then Drogheda can become a more attractive and desirable place in which to live.



Fig. 5. 16: Green Space in front of Buttergate which forms part of the Medieval wall complex and has been recently conserved to a high standard. This is an important space along a primary entrance to Drogheda, giving an early impression of Drogheda to the new visitor. It also sits below Millmount and contributes to a number of strategic vistas and views.



Fig. 5. 16: The Dale: Important for its natural heritage qualities, The Dale is also a long standing pedestrian route into Drogheda. In augmenting its linear park/cycle route potential, making it safe, easy and pleasant to use, care is needed to also maintain and enhance its biodiversity.



Fig. 5. 17: St. Mary's Cemetery: This is a large green space with potential for improved connection to The Dale. It is an oasis of calm with dramatic views towards Drogheda, a somewhat hidden treasure within the Heritage Quarter. Public access is informally available and this needs to be monitored and managed to respect its heritage and spiritual values (as a graveyard), while allowing for wider enjoyment of its significant amenities.



Fig. 5. 18: Front gardens. These privately owned spaces make a valuable contribution to the overall amenity of Drogheda, in particular when well maintained as here. The community benefit which accrues from private endeavour is one of the considerable attributes of urban living.

Place / Activity GREEN SPACES	1.St Mary's Cemetery	2.The Dale	3.Buttergate	4.Marsh at Viaduct	5.North Strand Esplanade	6.Cord Burial Ground	7. Sienna Convent & Cemetery	8.St Peter's Graveyard	9.Magdalene St/Rope walk space	10.Bolton Gate Space	11.Marion Close	12 Front gardens	13.Private Gardens	Place / Activity
LINK/PLACE	Place	Link/Place	Link	Place	Link/Place	Place	Link/Place	Place	Link/Place	Place	Link/Place	Place	Place	LINK/PLACE
EVENTS														
Public Event	No	No	No	No	No	No	No	No	No	No	No	No	No	Public Event
Market / stalls	No	No	No	No	No	No	No	No	No	No	No	No	No	Market / stalls
AMBIENCE														
Heritage Setting	Good	Good	Poor	Good	Poor	Good	Good	Good	Poor	Poor	Poor	Good	Good	Heritage Setting
Views	Good	Good	Good	Good	Good	Good	Good	Good	Poor	Good	Good	Good	Good	Views
Microclimate	Good	Good	Good	Poor	Poor	Good	Good	Good	Good	Poor	Good	Good	Good	Microclimate
Feature / Sculpture	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	Yes	Yes	Feature / Sculpture
Nighttime	No	No	No	No	No	No	No	No	No	No	No	No	No	Nighttime
Information	Yes	No	No	No	No	No	No	Yes	No	No	No	No	No	Information
ACTIVITY														
Interactive point	No	No	No	No	No	No	No	No	No	No	No	No	No	Interactive point
Recreation/Play	No	No	No	No	Yes	No	No	No	No	No	No	No	No	Recreation/Play
Seating	No	No	No	No	Yes	Yes?	No	No	No	No	No	No	No	Seating
Furniture	Poor	Poor	Poor	Poor			Poor	Good	Poor	Poor	Poor	No	No	Furniture
Hangout	No	No	No	No	Yes	No	No	No	No	No	No	No	No	Hangout
Social	No	No	No	No	Yes	No	Yes	Yes	No	No	No	Yes	Yes	Social
MOVEMENT														
Public transport	No	No	Yes	No	No	No	No	No	No	Yes?	No	No	No	Public transport
Drop-off Point	No	No	No	No	No	Yes	Yes	Yes	Yes	No	No	No	No	Drop-off Point
Cycle racks	No	No	No	No	No	No	No	No	No	No	No	No	No	Cycle racks
Cycle	No	No	No	No	No	No	No	No	No	No	No	No	No	Cycle
Pedestrian Accessibility	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	Poor	No	No	Pedestrian Accessibility
NATURE														
Biodiversity	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Good	Good	Good	Biodiversity
Lawn	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Lawn
Planting	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Planting
Trees	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Trees

Shaded categories indicate potential for change or improvement.

Table 5.3: Key Green Spaces Analysis



Map 5.5: Areas within 300m of open spaces suitable for play

Play Maps

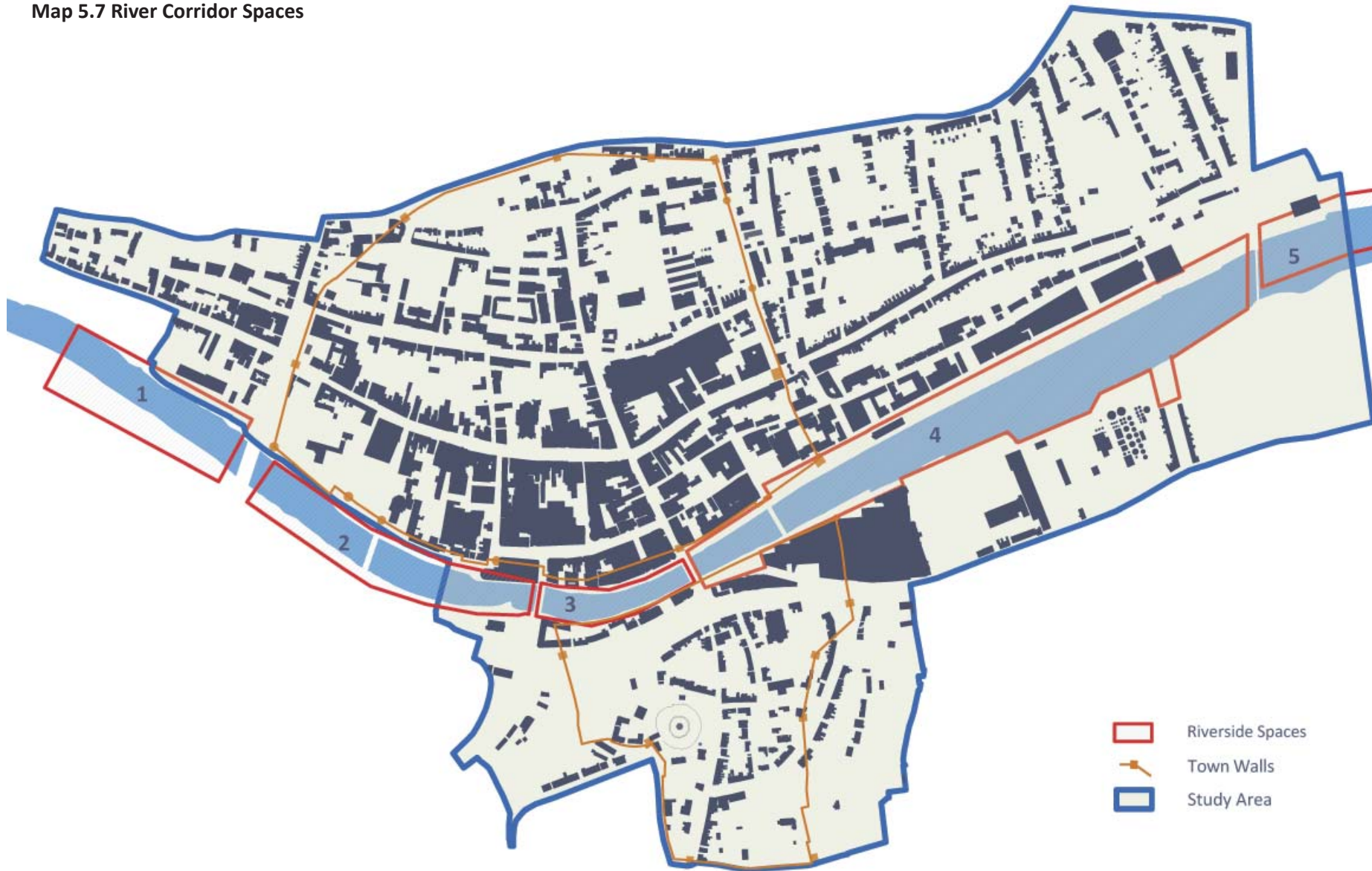
The play maps (5.5 and 5.6) identify the public open spaces that are readily accessible in Drogheda. The radii drawn from these spaces represents 300m zone, which is a reasonable distance for children to travel to reach them. The maps do not take into account the barriers that may exist, particularly roads, or poor footpaths, to reaching the spaces.

Whilst there appears to be plenty of open spaces across Drogheda, they are limited in heart of the heritage quarter. There are also private and semi public spaces (included in Map 5.5 and omitted from Map 5.6) which are not suitable for full access, or for active recreation, such as cemeteries. This indicates that the heritage quarter would benefit from increasing the number or quality of public open spaces for various types of recreation, amenity and play. This is especially important in attracting people to live in the area, but will also benefit people who work in, or visit the heritage quarter. Where this is not possible, or where spaces are limited, barriers to access of public open spaces should be eased by, for instance, good wayfinding and signage, improved routes to spaces and road crossings. Existing public open spaces within or adjacent to the heritage quarter could be improved in the type and variety of amenity that they offer, taking into account play opportunities, natural and cultural heritage and accessibility.



Map 5.6 Areas within 300m of open spaces suitable for play - publicly accessible spaces only

Map 5.7 River Corridor Spaces



- 1: West of Peace Bridge
- 2: Peace Bridge to Haymarket Bridge
- 3: Haymarket Bridge to St. Mary's Bridge (Bull Ring)
- 4: St. Mary's Bridge (Bull Ring) to Boyne Viaduct
- 5: East of Boyne Viaduct

Place / Activity RIVER SPACES	1.West Of Boyne Road Bridge	2.Boyne Bridge to Haymarket Bridge	3.Haymarket Bridge to Bullring Bridge	4.Bullring Bridge to Mall	5.Mall to Viaduct	6.Viaduct to east	Place / Activity
EVENTS							EVENTS
Public Event	No	No	No	No	Yes	No	Public Event
Market / stalls	No	No	No	No	No	No	Market / stalls
AMBIENCE							AMBIENCE
Heritage/Setting	Good	Poor	Poor	Good	Good	Good	Heritage/Setting
Views	Good	Good	Poor	Good	Good	Good	Views
Microclimate	Poor	Poor	Poor	Good	Poor	Poor	Microclimate
Feature / Sculpture	No	Yes	No	Yes	Yes	Yes	Feature / Sculpture
Nighttime	No	No	Yes	Yes	No	No	Nighttime
Information	No	Yes	No	Yes	No	No	Information
ACTIVITY							ACTIVITY
Interactive point	No	No	No	No	No	No	Interactive point
Recreation/Play	Yes	No	No	No	No	No	Recreation/Play
Water based Recreation/Play	Yes	No	No	No	No	No	Water based Recreation/Play
Seating	Yes	No	Yes	Yes	No	Yes	Seating
Furniture	Good	Poor	Poor	Good	Poor	Poor	Furniture
Hangout	Yes	No	No	Yes	No	No	Hangout
Social	Yes	No	Yes	Yes	No	No	Social
MOVEMENT							MOVEMENT
Public transport	No	No	No	No	No	No	Public transport
Drop-off Point	Yes	Yes	No	Yes	Yes	No	Drop-off Point
Cycle racks	No	No	No	No	No	No	Cycle racks
Cycle	No	No	No	No	No	No	Cycle
Pedestrian Accessibility	Poor	Poor	Poor	Poor	Poor	Poor	Pedestrian Accessibility
Access to River	Good	Poor	Poor	Poor	Poor	Poor	Access to River
NATURE							NATURE
Biodiversity	Good	Poor	Poor	Poor	Poor	Good	Biodiversity
Lawn	Yes	No	No	No	No	Yes	Lawn
Planting	Yes	Yes	Yes	Yes	No	Yes	Planting
Trees	Yes	Yes	No	Yes	No	Yes	Trees

Shaded categories indicate potential for change or improvement.

Table 5.4: River Spaces Analysis



Fig. 5.19 Surviving limestone flag and pavement grille within modern pavement.



Fig. 5. 20 Limestone kerbing and cobble drain



Fig. 5.21 Milestone against rubble boundary wall to Cord Cemetery, Cord Road.



Fig. 5. 22 Fairgreen: Cobble drain and iron gully



Fig. 5. 23 iron pavement grille, basement area, Fair St



Fig. 5. 24: Cobble drain, Millmount

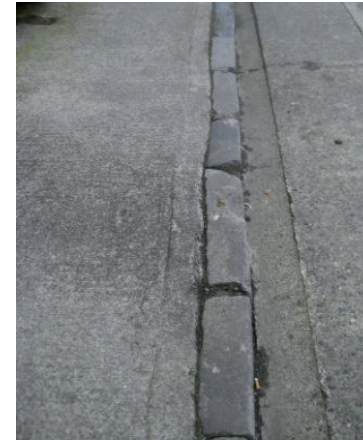


Fig. 5. 25: Limestone kerbing at Green Lanes

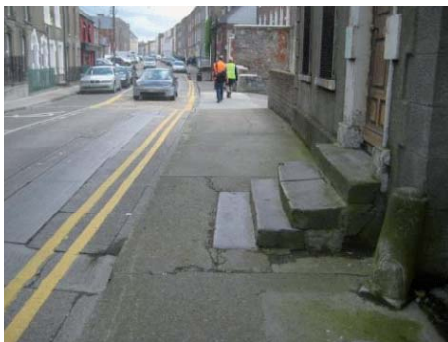


Fig. 5.26. Limestone steps - these present a challenge for access along pavement and to building. Fair



Fig. 5. 27 Limestone base to pillar at Drogheda Borough Offices entrance

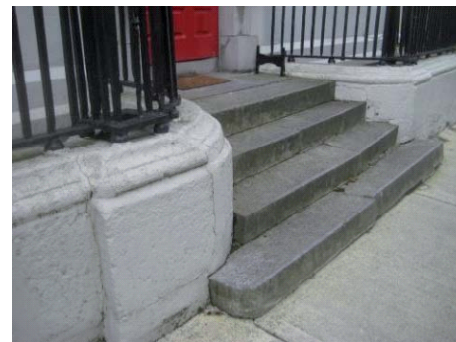


Fig. 5. 28 Fair St., Impressive entrance with limestone steps, wrought iron railed enclosure on painted stone plinth to basement area



Fig. 5. 29 Coal hole in pavement indicates basement cellar below. Limestone kerbing survives also. Many of these are found throughout the Heritage Quarter.

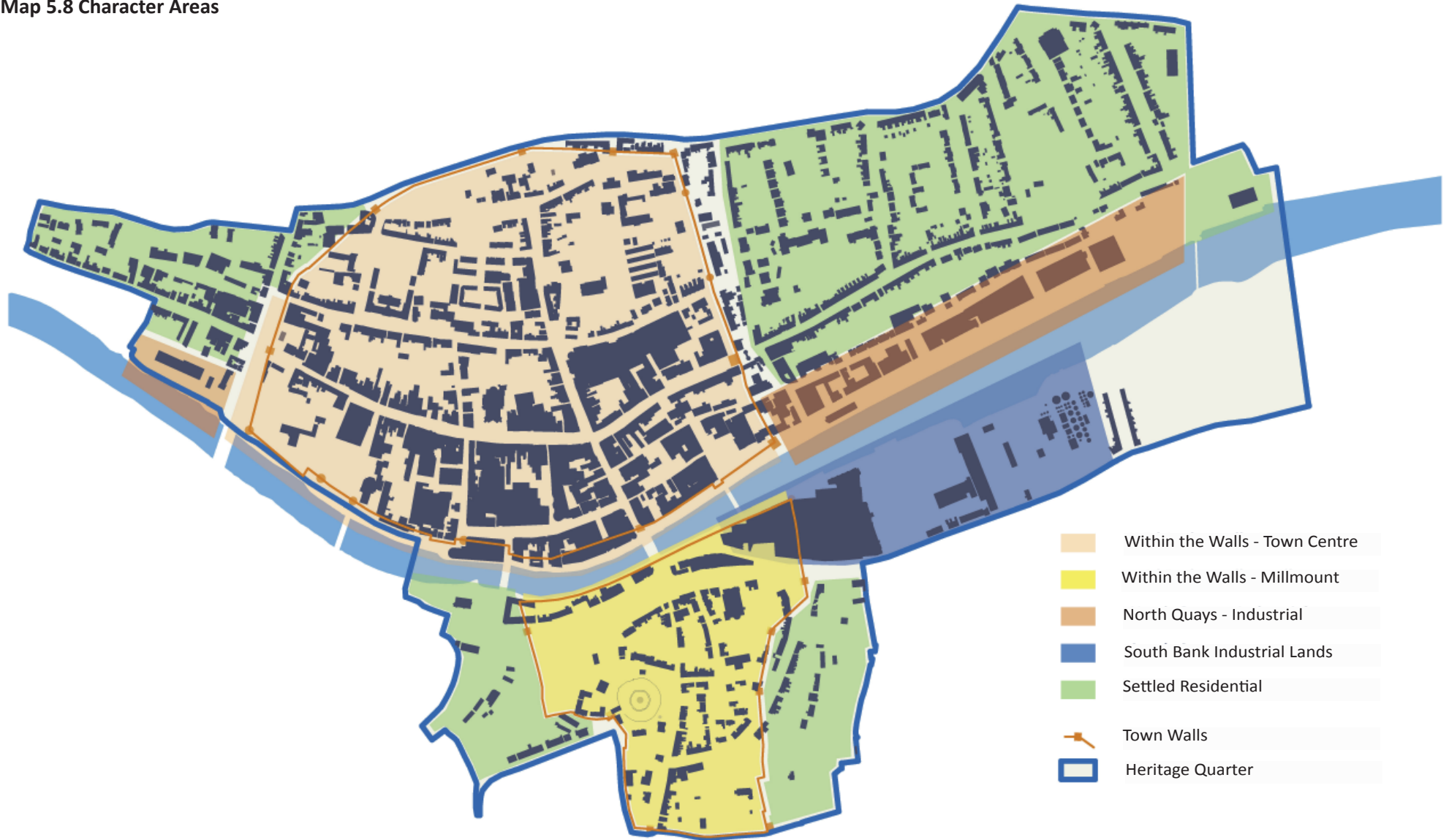
Historic Elements of Public Realm

Throughout the Heritage Quarter there survives many historic items of street paving and furniture - kerbing; drainage channels; ironworks; milestones; stone walls, etc. These survive as isolated fragments - an limestone border to a former basement area, now infilled with an iron pavement grille - and as intact elements such as the substantial entrance steps and wrought iron railings to a former street residence.

These elements are of value for several reasons. They communicate a sense of history and former use - they catch the eye and prompt the mind to consider former occupation and ways of living. They are often (mostly) made of quality materials and are well made - to replace them would be, at least, costly. In many cases they continue to function as originally intended, as boundary walls, as kerbs, as entrances. Above all, they are distinctive to Drogheda and therefore contribute to its unique identity.

Where carrying out public realm improvements, these elements should be retained - preferably in their current location. Where this is not possible, they should be retained and reused elsewhere within the Heritage Quarter (See section 6.4 for further guidance on historic street elements).

Map 5.8 Character Areas



- Within the Walls - Town Centre
- Within the Walls - Millmount
- North Quays - Industrial
- South Bank Industrial Lands
- Settled Residential
- Town Walls
- Heritage Quarter

Character Areas

Map 5.8 identifies a number of character areas within the Heritage Quarter. The character of these areas is distinguished by the prevailing scale, built character and land uses. Some characteristics are shared by more than one area, and the boundaries between some areas can also overlap. However, in the main, there is a distinctive quality to each of the five character areas as described below, which is useful to consider when developing a policy on scale for the Heritage Quarter:

1. Within the Walls - Town Centre: This area comprises the principal commercial and civic heart of Drogheda. It contains fine grain and larger footprint/scale buildings. It is defined by the line of the northern medieval town wall enclosure inside which the prevailing scale of buildings has been and remains, mostly, larger than those outside this boundary. Therefore, while only localised sections of upstanding walls remains, the line of the historic walls can be read (traced) by this change in scale.

Prevailing heights range between 3 and 4 storeys (historically) with a number of recent buildings at 5 to 6 storeys. The skyline is also punctuated by the spires and cupolas of prominent church and civic buildings.

2. Within the Walls - Millmount: The former military fortification at Millmount gives its name not just to this historic complex, but also to the neighbourhood, which is primarily low in height and residential in character, except along the river edge where higher scale commercial/retail functions are more characteristic of the northern side of the walled town, this part formerly relating to the port and industrial functions.

Prevailing historic height of this area is 2 storeys with some 3 storey and large footprint buildings, such as St. Mary's R.C. church and, just outside the area, St. Mary's convent. The more recent Scotch Hall and former NIB buildings have again raised the scale, more in keeping with the former industrial buildings which ran along the South Bank Industrial lands.

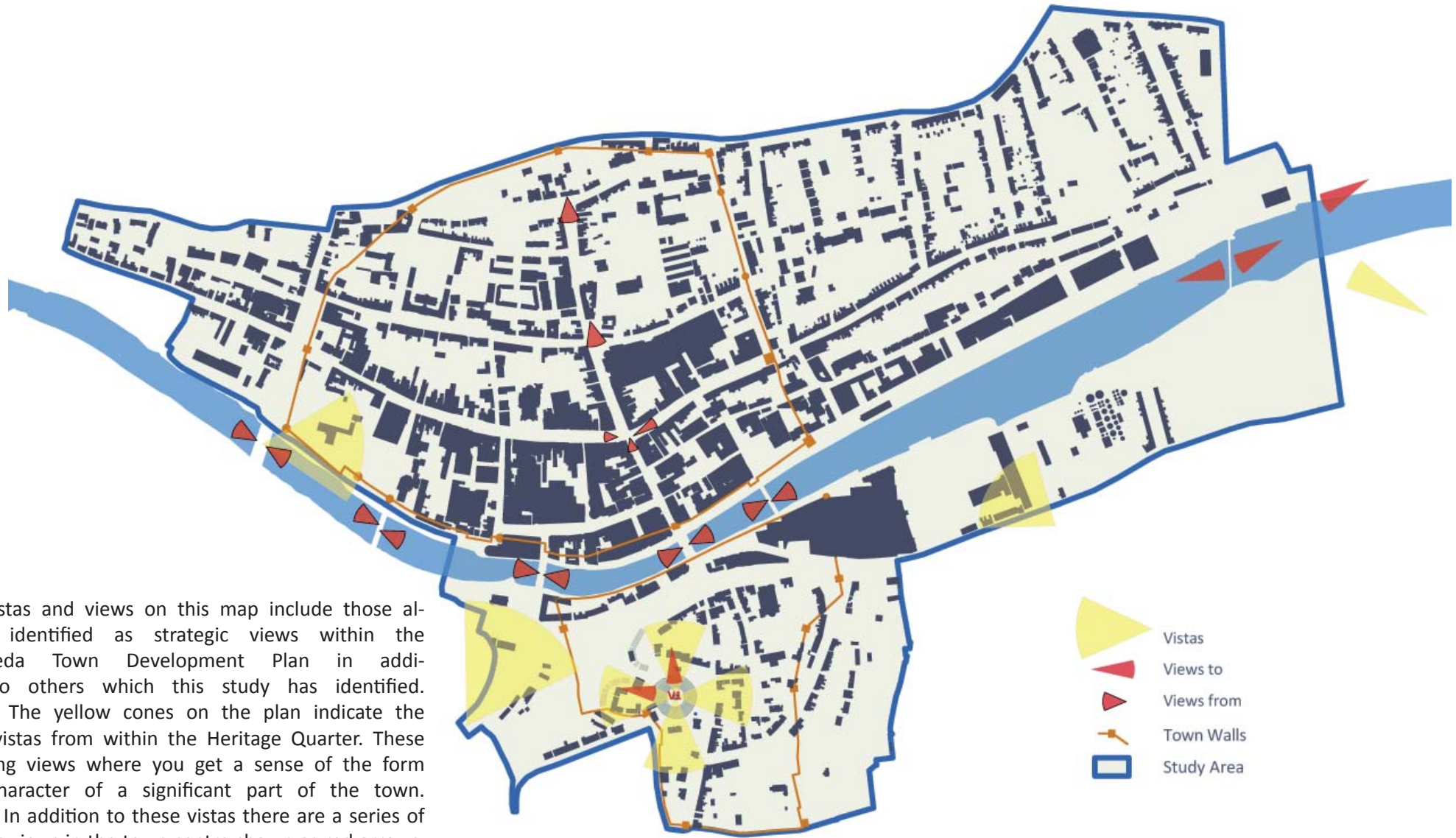
3. North Quays: The historic docklands provide a relatively consistent 4 and 5 storey height which also includes single and 6 storey buildings set back from the river edge to form a generous working space, along which are a number of single storey functioning stores. Building footprints are mostly larger than within the walled town, reflecting the industrial warehouse typology of much of the buildings.

4. South Bank Industrial Lands: Most of the historic industrial buildings no longer survive, as a comparison of the 1911 and current OS maps show. However, the recent Scotch Hall/D Hotel development, part of which lies in this area, sets a substantial contemporary height of 5 storeys with a 7 storey set back and imposes a significant footprint. Scotch Hall typifies the modern 'groundscraper' of the Irish town context. However this quality is tempered by its location outside (mostly) the walled town with good pedestrian connection to the historic town centre.

5. Settled Residential: The fifth character area is found in several locations at the outlying parts of the Heritage Quarter. Primarily residential, although also some institutional uses and ancillary services uses such as motor repair workshops. Prevailing height is 2 storeys with a notable exception, being the former Sienna Convent. The garden is a notable feature of the individual plot.

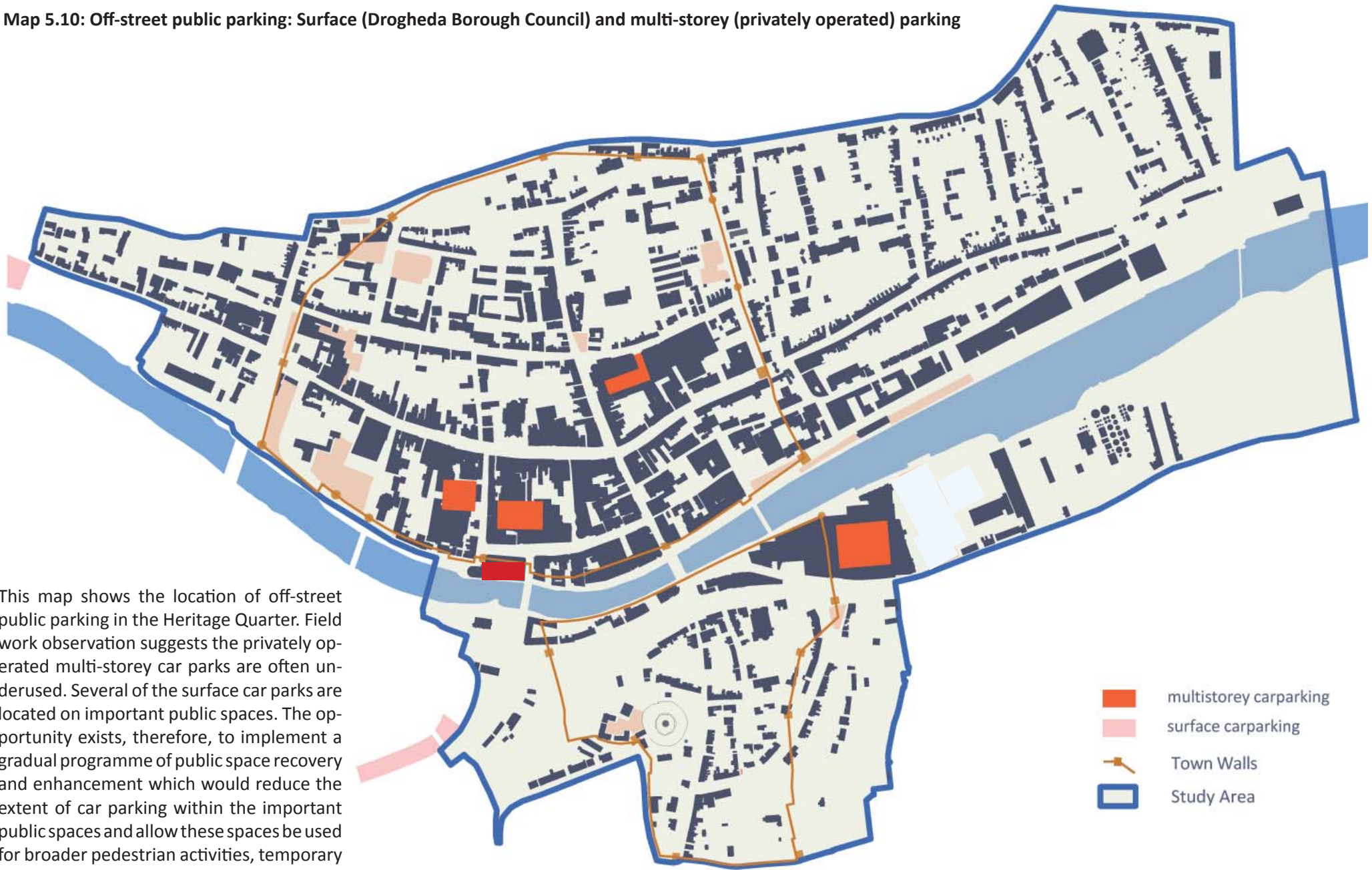
The Character Areas described above are important in considering the appropriate scale of new development in Drogheda and are used to inform guidance on scale which is set out in section 6.5 of the Framework Plan.

Map 5.9: Views and Vistas within the Heritage Quarter



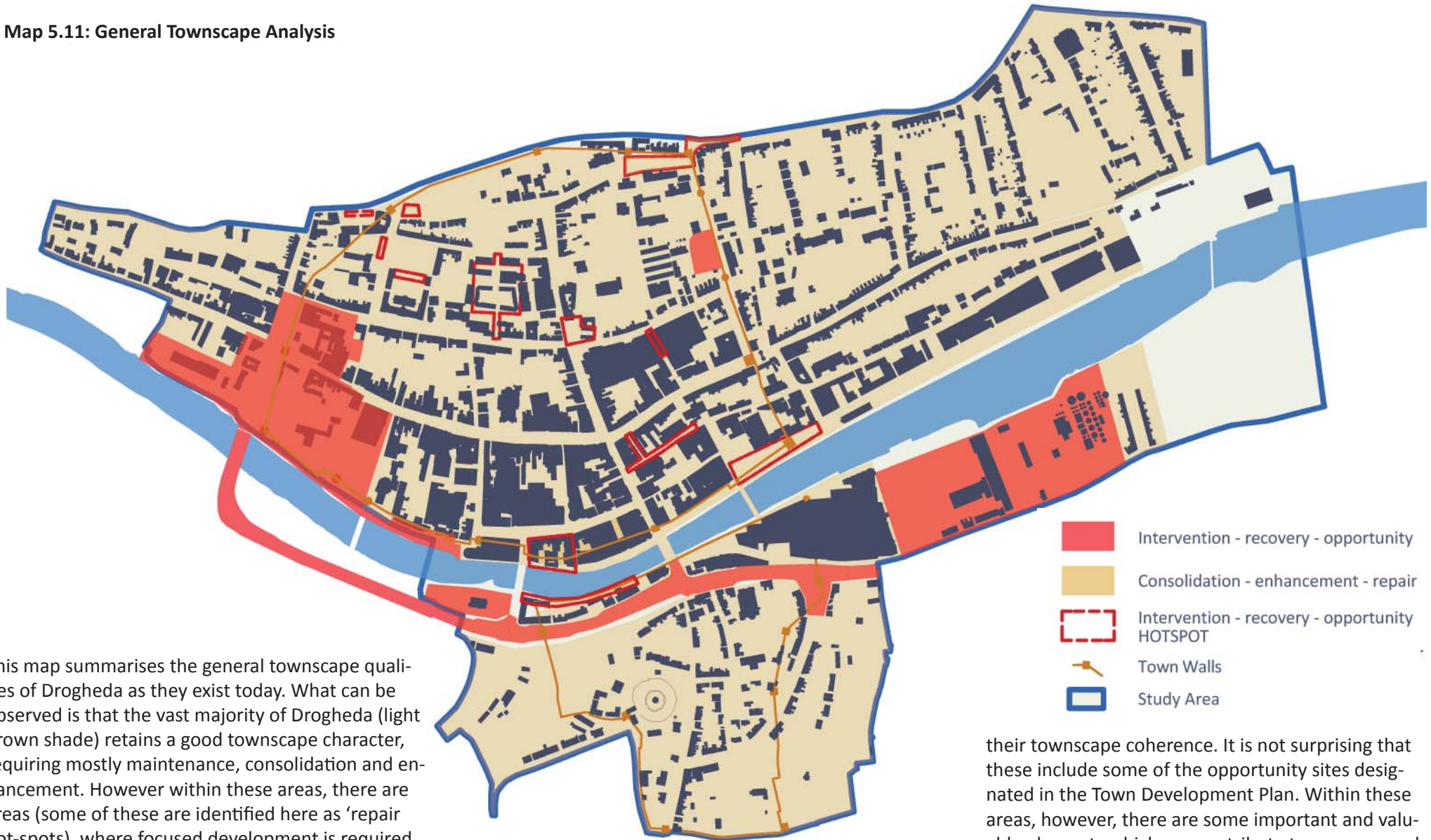
The vistas and views on this map include those already identified as strategic views within the Drogheda Town Development Plan in addition to others which this study has identified. Vistas: The yellow cones on the plan indicate the main vistas from within the Heritage Quarter. These are long views where you get a sense of the form and character of a significant part of the town. Views: In addition to these vistas there are a series of specific views in the town centre shown as red arrows. These are generally views along a street or framed between buildings towards one of the town landmarks. It is important that these views are not blocked. However it is just as important that taller structures do not intrude into the background of these views.

Map 5.10: Off-street public parking: Surface (Drogheda Borough Council) and multi-storey (privately operated) parking



This map shows the location of off-street public parking in the Heritage Quarter. Field work observation suggests the privately operated multi-storey car parks are often underused. Several of the surface car parks are located on important public spaces. The opportunity exists, therefore, to implement a gradual programme of public space recovery and enhancement which would reduce the extent of car parking within the important public spaces and allow these spaces be used for broader pedestrian activities, temporary events, etc.. Cities such as Copenhagen have successfully carried out such initiatives.

Map 5.11: General Townscape Analysis

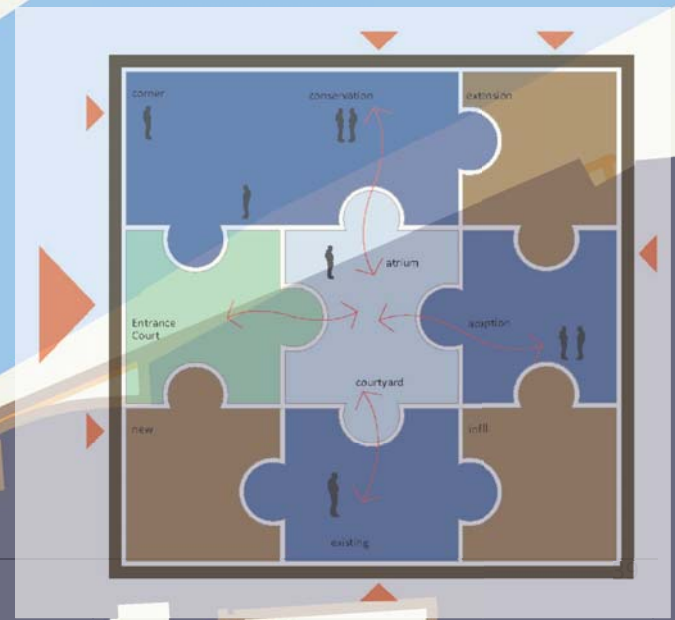


This map summarises the general townscape qualities of Drogheda as they exist today. What can be observed is that the vast majority of Drogheda (light brown shade) retains a good townscape character, requiring mostly maintenance, consolidation and enhancement. However within these areas, there are areas (some of these are identified here as ‘repair hot-spots’), where focused development is required. This ranges from bringing important buildings back into use (e.g. the St. John’s housing at St. Peter’s Hill) to public realm improvements (e.g., St. Peter’s Hill, Potato Market, The Mall). It is noticeable that the settled residential areas of Millmount and the

eastern and western housing north of the River, are all pleasant and well performing neighbourhoods (Millmount being noted as such at the stakeholder consultation workshops). The red shaded areas, on the other hand, require more focused intervention to recover

their townscape coherence. It is not surprising that these include some of the opportunity sites designated in the Town Development Plan. Within these areas, however, there are some important and valuable elements which can contribute to recovery and should inform design proposals. Development Briefs and masterplans should be prepared for these areas to ensure integrated proposals which reflect the vision, objectives and guidance of this Framework Plan.

STRATEGY



6.0 The Framework Plan

6.1 Vision and Key Objectives

The Urban Design Framework Plan is based on a vision for Drogheda's Heritage Quarter which have been developed through on the ground observation, study, analysis and interpretation of existing conditions and prevailing relevant policy and guidance and, through local and stakeholder consultation. The vision can be articulated as follows:

Drogheda Heritage Quarter remains a distinctive urban centre, building on its recognised and memorable urban qualities, containing durable and enduring buildings and places - existing and new, is pleasant to dwell, work and spend time in and is active, diverse and culturally rich in character and function.

This vision can be realised through a range of objectives. These have been informed by the many related strategic objectives and policies of the Drogheda Borough Development Plan 2011-2017. They are also informed by study of the town's qualities and challenges, regard to contemporary approaches to urban design within historic urban areas and input from Steering Group and through consultation during the Framework Plan preparation process. The objectives below range from general to specific

- Drogheda's Heritage Quarter will provide the range of uses, facilities and amenities relevant to a contemporary urban centre in a manner which is consistent with the architectural qualities of the town.
- Enhancement of a number of significant public spaces within the Heritage Quarter, including activation and integration of network of historic lanes. These will build on distinctive qualities, integrating historic fabric of value and provide a diversity of space and function to residents and visitors.
- Recover and present the historic line of the Medieval walls.
- Support the maintenance and improvement of historic buildings within the Heritage Quarter including full and active use of all floors. New buildings to achieve a high architectural and material quality, addressing context in sensitive ways which contribute to place making and are of enduring physical and architectural quality over time.
- Heritage Quarter as main focal point of town - the living room of Drogheda. As such it is a social place, a cultural place, a place of business and of respite.
- Drogheda as a place for quality living, homes located strategically and in clusters/pockets to facilitate community and neighbourhood building. Wider planning strategies to ensure a sufficient, mixed and settled residential population within the urban hinterland to create a sustainable urban population to support and develop Drogheda's amenities and services.
- The wider setting of Drogheda within the Eastern Corridor provides a potential symbiosis - between the town serving the wider population which both needs and 'feeds' the town. As such, Drogheda town centre/Heritage Quarter to support a range of facilities and amenities greater than expected for a town of its (physical) size.

The Framework Plan sets out guidance under five principal headings:

- 6.2 Existing Buildings
- 6.3 New Development
- 6.4 Living in the Heritage Quarter
- 6.5 Scale of Development
- 6.6 Public Realm

Guidance is given in general descriptive and diagrammatic form. Where appropriate examples of similar type development approaches from other locations are provided.

Section 6.5 addresses the issue of scale (height and mass). It refers to the analysis provided in sections 2.0 and 5.0 in developing general parameters for scale of new buildings, or vertical extensions to existing.

As a means of 'testing' the guidance and to assist in visualising how the guidelines might be translated into actual built form and new/recovered public spaces, **section 6.7** sets out a number of '**scenario schematic proposals**', in the form of sketch plans and diagrams, based on sample sites within the Drogheda Heritage Quarter. The sample sites were identified due to a number of factors - previous/future development pressures; problems of underuse/dereliction; strategic importance and development potential.

Development within Drogheda will take place at several scales and may range from improvement works to existing buildings to large scale development comprising new and existing buildings as well as public spaces and routes. Development will also be carried out privately and by the local authority.

6.2 Existing Buildings

A whole range of building qualities exist within the Heritage Quarter. Some are acknowledged for their architectural heritage quality through statutory protection and so development, alteration and use will be guided by existing statutory guidance (DAHG Architectural Heritage Protection Guidelines for Protected Structures). In this Framework Plan their immense value to the particular character of Drogheda is acknowledged and so their continued use, maintenance and contribution to Drogheda Heritage Quarter must be encouraged and facilitated as a priority.

In the Heritage Quarter there are many existing buildings which are not protected and which make different impacts on the town's character and use. Some lie within the ACA's and, while perhaps of local value and not appropriate for statutory protection, they do make an important contribution to the area's distinctive character and townscape. Their retention and continued use is important.

In addition to the former there are also many ordinary buildings which are of no special architectural merit, but which maintain the streetscape - building line and grain - and accommodate, or have the potential to house, a range of uses important to the town's vitality, culture and economy. The term 'mural buildings' has been used for this type of building which forms part of the streets' background structure. The retention of these buildings might be based on a range of, generally non-aesthetic reasons, but for socio-economic or wider sustainability (embodied energy) reasons. However, the replacement of such buildings will, in principle, be acceptable. What is of concern is the nature of any replacement and guidance for this is addressed under New Development at 6.3 below. Alternatively such buildings may be suitable for extensive modification and adaptation, in particular their primary facades.

Finally, there are a number of existing buildings within the Heritage Quarter which have a negative impact on their immediate context and which are also likely to be of a typology and construction which is difficult to adapt, or alter, economically. Their replacement should be considered acceptable, indeed desirable.



Figs. 6.1 and 6.2: Photographs show four historic buildings in Amsterdam connected to accommodate a music store. The connection is discreet, yet the identity of the shop is clear and the historic grain is maintained.

6.2.1 Guidance on Interventions to Existing Buildings: – Interventions, Extensions, Alterations.

All existing buildings, whether statutorily protected or the very ordinary are capable of alteration, intervention and extension. What is important is that the approaches to such change are carried out in a manner which adds architectural/urban value to the relevant building and its immediate context.

One of the objectives of the Urban Design Framework Plan is to ensure Drogheda's Heritage Quarter can provide the range of uses, facilities and amenities relevant to a contemporary urban centre in a manner which is consistent with the architectural qualities of the town. This can mean the need to provide larger floorplates than the traditional plot

building. In many instances it will be acceptable to connect, amalgamate existing buildings which are not protected.

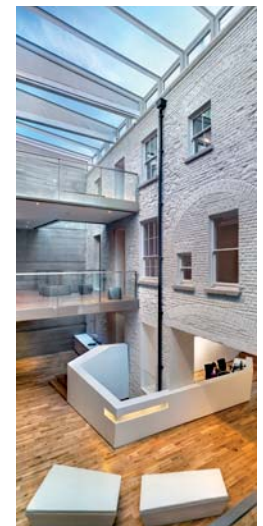
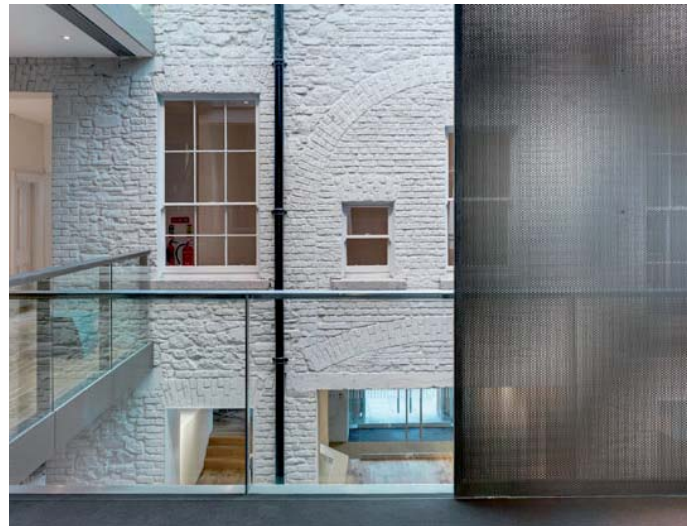
In some instances, it may also be appropriate to connect protected structures, but this will depend on the particular character of the building and the detail design of the intervention required for connection. For example, will important details such as decorative cornices be compromised, damaged or lost; what are the impacts of accommodating level differences on the architectural quality and plan-form of the interiors?

Alternatively extensions (principally to the rear) may be appropriate to accommodate modern services such as lifts, as well as providing extra space. And, a combination of integration of buildings and extension can be carried out successfully to achieve a greater accommodation. What is important is a clarity of architectural character; integration with the existing streetscape grain (especially within the ACA's and primary streetscapes) and an overall architectural integrity and coherence. Such significant alterations will be required to provide uses for all floor levels, onto public streets (ie no storage/empty upper levels).



Figs 6.3, 6.4, 6.5 and 6.6: Refurbishment and adaptation of five protected structures to accommodate Dublin Dental Hospital. This involved the interconnection of the five protected structures with a roof extension which accommodates a research/study area. What makes this approach acceptable is the quality of the architecture - informed by rigorous research and understanding of the buildings - quality of detailing and materials and, to a large extent, the nature of the building's function (single use, teaching accommodation for the Dental Hospital). The interconnection of these buildings occurs at all floor levels and involved overcoming level differences. Such an approach shows how quite radical intervention can be carried out while maintaining overall integrity and particularly with regard to the streetscape character.

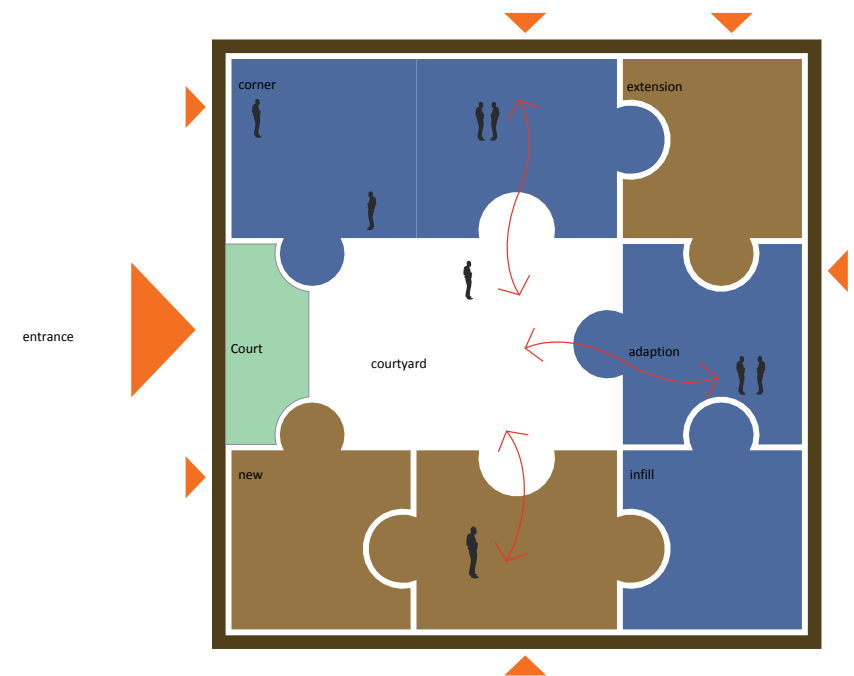
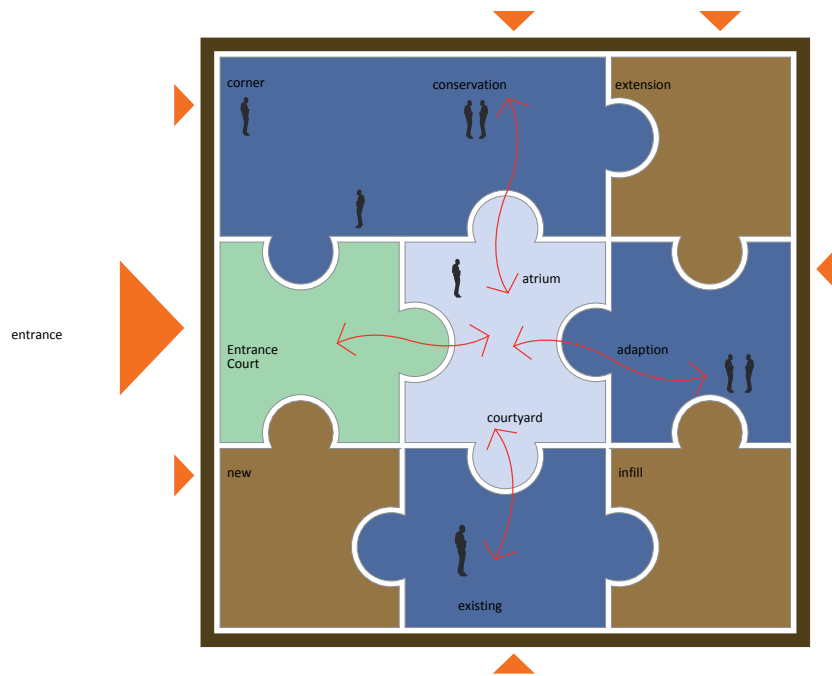
Materials for extensions: For repairs to historic buildings, and in particular for protected structures and buildings of traditional construction, materials used will be compatible with the historic building and its construction methodology. This is especially so where carrying out works to enhance energy efficiency. Priority to be given to the use of traditional materials (however these can be used to give contemporary expression to new interventions). Materials and finishes should be proposed for their durability over time and be appropriate to the architectural expression and civic importance of the building. This guidance must be read in conjunction with Drogheda Borough Council Development Plan 2011-2017.



Scale of extensions: Additions to existing buildings to address the particular scale of the existing building. This does not necessarily mean additions must be similar in scale, but that there should be a coherent relationship between the form, massing and

proportion of the existing building and any additions. Where large roof/vertical extensions are proposed to existing large scale modern buildings, consideration should be given to improving other parts of the building, in particular ground level/street animation and elevational treatments.

Figs. 6.7, 6.8 & 6.9: Above, and right - internal images of rear extension to protected structure to accommodate offices. Connection is made via a rooflit atrium space which accommodates circulation. The rear facade of the protected structure is carefully repaired in a manner which allows the building's own history of development to be read.



Figs. 6.10 & 6.11: Diagrams showing principles behind amalgamation of existing and new buildings to provide opportunities for larger floorplates while respecting prevailing grain and form. The blue colour represents an existing building, the brown a new building. The use of courtyards/covered atria allow for light, ventilation and circulation.

6.3 New Development

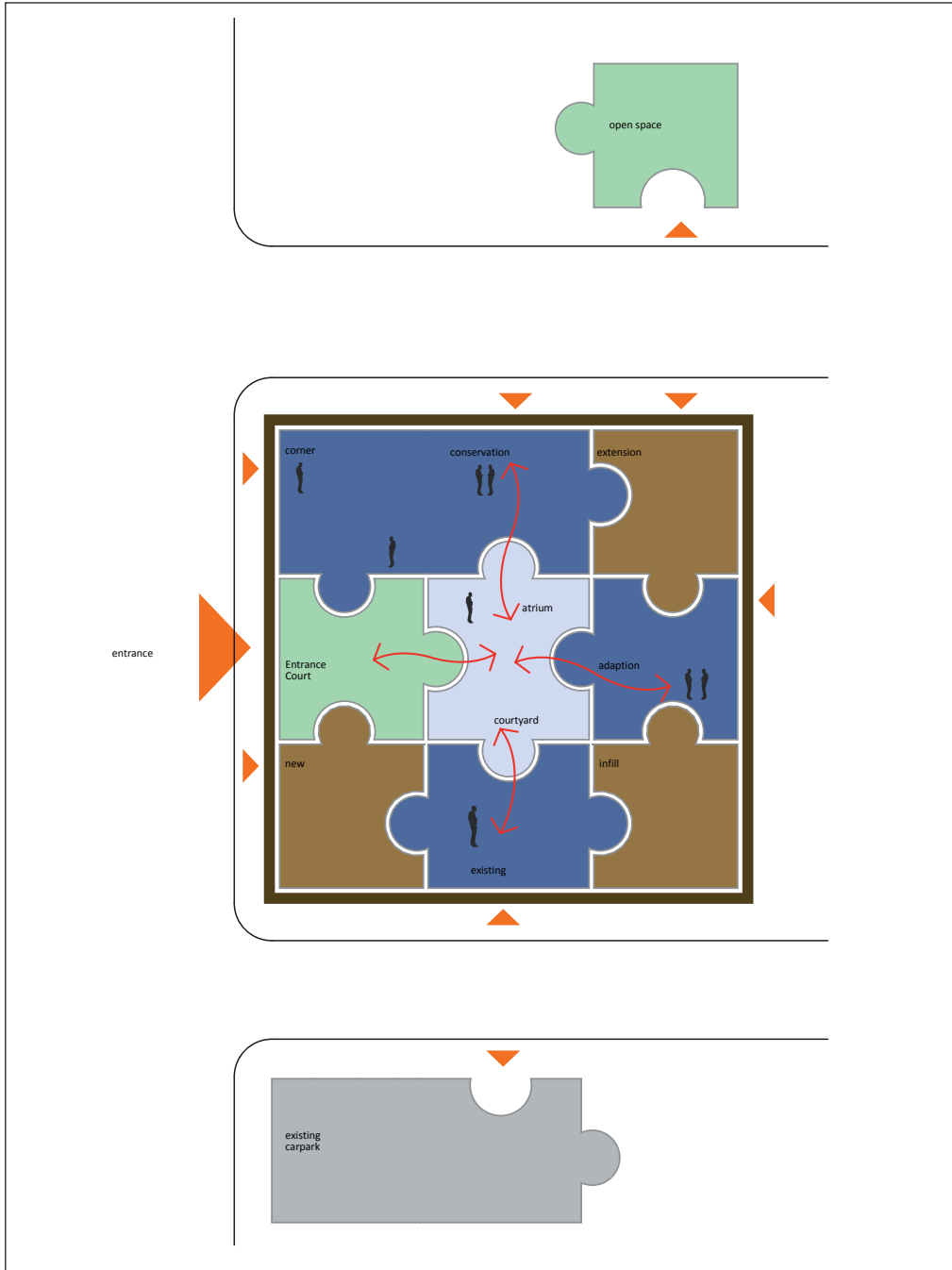
A range of types and scales:

- Infill – existing gap sites or replacing an existing building (may be in middle of streetscapes; corner sites or along lanes/smaller streets)
- Part of existing block amalgamation – new and existing – within an existing urban block; may also include backland developments (see diagrams above)
- Large urban block development
- Large scale individual buildings

There are likely to be fewer of the latter two examples, as wholesale demolition of existing blocks is unlikely. The Drogheda Borough Development Plan 2011-2017 has identified a number of urban blocks/opportunity sites where development is desirable as has the Drogheda Port LAP. The guidance in this plan is relevant to these sites and others within the Heritage Quarter.

Figs. 6.12, 6.13 & 6.14: Right & below historic warehouse on Mayoralty Street which has been adapted and integrated with other buildings to accommodate a range of uses off an internal street. The concept of this approach may be appropriate for other situations, where the interiors are not of such architectural significance to preclude intervention





Figs. 6.16. & 6.17: Above and right: Asprey Store, Bond Street London where 5 listed buildings are linked to the rear by a new glazed retail space which

Fig. 6.15 left: Diagram showing approach where proximity of existing off-street parking can alleviate requirement for new parking within development. It also shows where an adjacent existing open space could be upgraded as part of the develop-



Figs 6.18 & 6.19, above, are two examples of new corner infill development in Drogheda. Both examples create a corner mark and retain the street line and general grain



Fig. 6.20 Above a large new public building within an historic streetscape (Farnham Street, Cavan). This would represent a relatively large individual building.

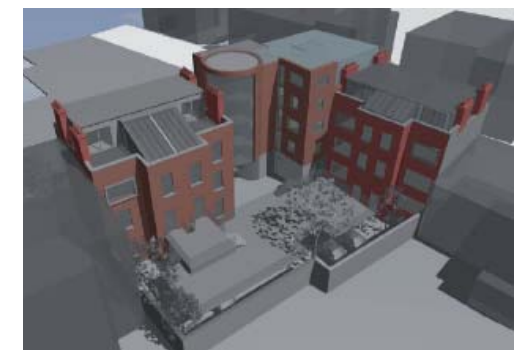


Fig. 6.21 Above is a series of design studies for a group of existing buildings, including protected structures (of which the corner building was badly fire damaged). These explore the retention and adaptation of all existing buildings as one option, with additional roof top story and, as a second option, the replacement of an existing pair of unprotected, derelict, buildings with a new 7 storey residential buildings. What is relevant, is that the higher building is located on a secondary street, away from the most sensitive main street. This higher structure also contains a single large apartment at each floor level, giving triple aspect living accommodation. The principle here was that the higher building would also be of higher quality and is located in a less sensitive location. The rear yard area was designed to accommodate a small amount of surface parking at ground level, with a raised terrace above, providing open space with southerly aspect, for the upper level apartments (most had their own balconies also). Ground floor is retail use (office use for the off-main street higher building).

6.3.1 Additional Guidance for new development

Depending on typology, scale and location, there will be different architectural and urban design requirements for new development to address. However, as a rule, all new development needs to respond to the following criteria which support existing guidance in the Town Development Plan and national guidance in the Urban Design Manual and, for new development within ACA's, the DAHG Guidelines on Architectural Heritage Protection:

- Making spaces / permeability – linkages within and without site: This is particularly relevant to larger scale developments which may comprise several plots or operate at urban block/cross block scale.
- Making street edges – active frontage and street lines; Generally it will be a requirement for new development to maintain the existing street line. This is particularly so along the primary urban spatial sequences (Chapter 2). Along primary retail streets ground floor uses shall facilitate an active and vibrant public realm. Secondary streets within the town centre can support less active uses, however along the lanes it is desirable that at least two units with active uses open onto each lane within the historic commercial centre (Within the Walls - Town Centre, Character Area as identified in Map 6.2).
- Setting up, or facilitating views and vistas: Map 5.9 identifies the strategic views and vistas. In addition to avoiding negative impacts on these, new development shall consider the potential to establish new views which can positively add to the character of Drogheda. These shall be identified as part of any planning application.
- Architectural Form and expression For most development - in particular development which is located onto the primary urban spatial sequences - it may be appropriate to follow the traditional form of buildings in Drogheda. This can be achieved adopting a contemporary expression. Adopting pastiche decoration should generally be avoided in new development. Pastiche occurs as historicist affection and as contemporary design cliché. Both are equally unacceptable in the historic Irish town and generally reflect poor design competence. For a town of such architectural distinction as Drogheda, such approaches are inappropriate. Adopting new architectural forms should reflect an important civic or cultural use and will generally be appropriate for individual buildings or for contrasting extensions to existing civic or cultural buildings. In designing or assessing new forms consideration of their impacts - positive and negative - on the town's skyline profile shall be taken into account.
- Construction and Materiality: The construction methodology as well as materials and finishes make a significant contribution to architectural quality. While planning applications generally require minimal construction detail, in sensitive areas, such as the primary spatial sequences; ACA's; and for larger scale development, development proposals should provide documentation on proposed construction methodologies at planning stage. Materials and finishes should be proposed for their durability over time and be appropriate to the architectural expression and civic importance of the building. This guidance must be read in conjunction with Drogheda Borough Council Development Plan 2011-2017.
- Uses: Uses will comply with the land use zoning objectives of the Town Development Plan. Uses should reinforce prevailing use character identified in the Character Areas (Map 5.8). In particular the following uses should be encouraged:
 - Active uses should be prioritised on the traditional retail streets and also located strategically along the lanes to provide passive surveillance.
 - Residential uses are appropriate for all areas of the Heritage Quarter, and section 6.4 provides more detail on the range of approaches to provision of quality and sustainable residential accommodation within the area.
 - Opportunities to provide uses which can enhance and support the Heritage Quarter as the civic, cultural, commercial and retail heart of Drogheda should be encouraged, so long as they comply with other guidance in this Framework Plan which is aimed at ensuring the particular architectural and urban qualities of the existing town centre is maintained.
 - Uses which support Drogheda's tourist potential, in particular its cultural heritage tourism, should be encouraged within the Heritage Quarter. Such uses should aim to activate and enhance the existing historic environment, including buildings, public realm, Medieval Walls, industrial heritage, river heritage.
 - Uses which provide improved recreational and leisure facilities. In particular such uses which can animate the riverside, paved and green spaces identified in Maps 5.3, 5.4 and 5.7.
 - Temporary uses should be facilitated for existing buildings and public spaces which can support other cultural, tourism and general public realm animation objectives, in addition to assisting in the reuse of vacant/underused buildings and traditional retail areas.

- Quality of public realm: Following considerations to be addressed (more detailed guidance provided in section 6.6):
 - How will proposed development enhance or reduce quality of daylight, sunlight, shelter from wind and any other micro-climate impacts, enjoyed by existing and/or proposed new public realm connected to or adjoining the proposed development? In assessing this, consideration of how public realm is currently used or proposed to be used to be addressed.
 - Paving materials and designs for public realm: Retain and integrate any historic elements of value. In historic areas, generally keep paving patterns simple and use quality finishes - stone (limestone and granite) or high quality concrete - neutral colours. For historic spaces within the primary urban spatial sequences, consideration to be given to natural stone of local (Irish provenance). For new public spaces outside of these more sensitive historic environments there may be opportunities to adopt more contemporary approaches to public realm design and opportunities for temporary interventions may allow for experimentation and playfulness.
 - Consider how people of all ages, genders, abilities and disabilities can use the public spaces and design for wide social inclusion.
 - Soft landscaping to consider enhancement of biodiversity, seasonal impacts, shade and impact on views/vistas and should not compete with architectural qualities of spaces within the primary spatial sequences.
 - Street furniture should be of high quality materials for durability and of simple design, avoiding pastiche. Generally these elements should not visually distract from, or compete with, the spatial/architectural qualities.
- Access, servicing, parking, bin storage: How are these essential aspects of most development to be provided? How successfully are they integrated within the existing urban context. Where integration may cause adverse impact on existing environment and amenities, can these functions be accommodated elsewhere without diminishing facilities or causing knock-on adversity?
- Archaeology: In addition to assessing potential impact of development on existing archaeology as already provided for in the Town Development Plan and National Monuments Acts, development proposals should indicate, where relevant, how the development proposes to actively integrate known archaeology. In particular, opportunities to present the Medieval Walls in a positive manner and which can assist achieving objectives and policies of the Town Walls Conservation Plan, are to be implemented.
- How does development add to the existing context, in particular the immediate buildings? In particular for larger scale development, proposals should indicate how the existing environment will be enhanced by the development, in particular where existing vacant/underused buildings can be brought back into active, sustainable use. Equally, opportunities to improve the quality of street level frontages and uses which might enhance cultural facilities should be considered, or, opportunities to activate/animate upper floors with appropriate uses.
- Environmental / Sustainability issues: National standards for sustainability and energy performance are increasing all the time. Under the EU Energy Directive, It is now a requirement for all developments to provide a sustainability impact statement in advance of construction. While it is imperative to meet these objectives to improve energy performance and minimise dependence on fossil fuels, this must be balanced against potential conflicts with urban and building conservation objectives. Determining this balance requires ultimate consideration of providing quality human environments. Improvements may sometimes be best achieved at the macro scale, thus avoiding adverse impact on the individual building. Macro scale initiatives will require local, regional and national government support. For the individual building, very high standards can be achieved in new development, for existing buildings care must be taken to adopt methodologies to improve energy efficiency which may be entirely inappropriate to the particular building construction typology. Holistic consideration of impact on building must be taken into consideration.
- Biodiversity and Green Infrastructure Enhancement: What provisions are made to accommodate and enhance these important qualities? Consideration of Drogheda's Biodiversity Plan and national guidance on green infrastructure to be considered in all new development. Impacts on the SAC will require Scoping for Appropriate Assessment for most development applications within the Heritage Quarter, due to the proximity of the designated conservation area. The Boyne SAC should be positively planned for, as with the archaeological richness of the town - these are assets for the town and, appropriately managed, will add to the attractiveness of Drogheda for visitor and resident.

Guidance on scale of new development is addressed separately in section 6.5.

6.4 Living in the Heritage Quarter

Note on Residential Standards and Design Guidance

A significant element of any future development or regeneration strategy for the Town Centre of Drogheda will be the provision of a quantum of sustainable residential accommodation.

This will entail the need for providing, not just for smaller rental, student or social accommodation, but, more importantly, ensuring that owner-occupier housing suitable for rearing families who can be attracted in to quality developments within the town centre, in a manner that does not conflict with the existing historic fabric of the town.

This principle should apply to any strategy for upgrading existing buildings, infilling or stitching back smaller derelict sites into the urban fabric, or comprehensive developments on larger, cleared, development sites.

6.4.1 Residential in Existing Buildings:

The opportunities for improving the residential stock within the existing buildings in Drogheda, fall into three categories:

1. The conversion of the upper floors of retail units, or retail services units within the important commercial streets in the town
2. The upgrading of existing, under-sized, under-occupied terraced housing which retains a distinctive urban quality within the town
3. The re-imagining of former industrial or mill buildings which might be suitable or appropriate for conversion to residential use.



1. Living Over the Shop



2. Existing Housing Stock



3. Unused former industrial

1. 'Living Over the Shop'

There are some very fine residential building typologies within the Heritage Quarter. Many buildings, originally residential, have been converted to other uses or, sadly, lie empty. This is particularly so for the upper floors along the main retail/commercial streets and is typical of many Irish towns at present.

Encouraging people to 'live over the shop' remains a challenging objective for many town centres, however it continues to be recognised as an essential component of sustainable development. The concept of the compact urban centre is promoted in international and national policy. Recent initiatives such as The Living City (Finance Act 2012) and the Historic Townscape Initiative - both at pilot stage when this Framework Plan has been written - are aimed at supporting urban living in historic Irish towns.

In Drogheda, the network of lanes can assist activation of upper lanes.

The diagram on the following page illustrates some approaches to adapting existing street buildings for 'living over the shop'. The key principle is to ensure a quality residential environment, making best use of the existing plan form, architectural detail and spatial qualities. In this way, it is preferable that larger sized residential units are provided for living over the shop units.

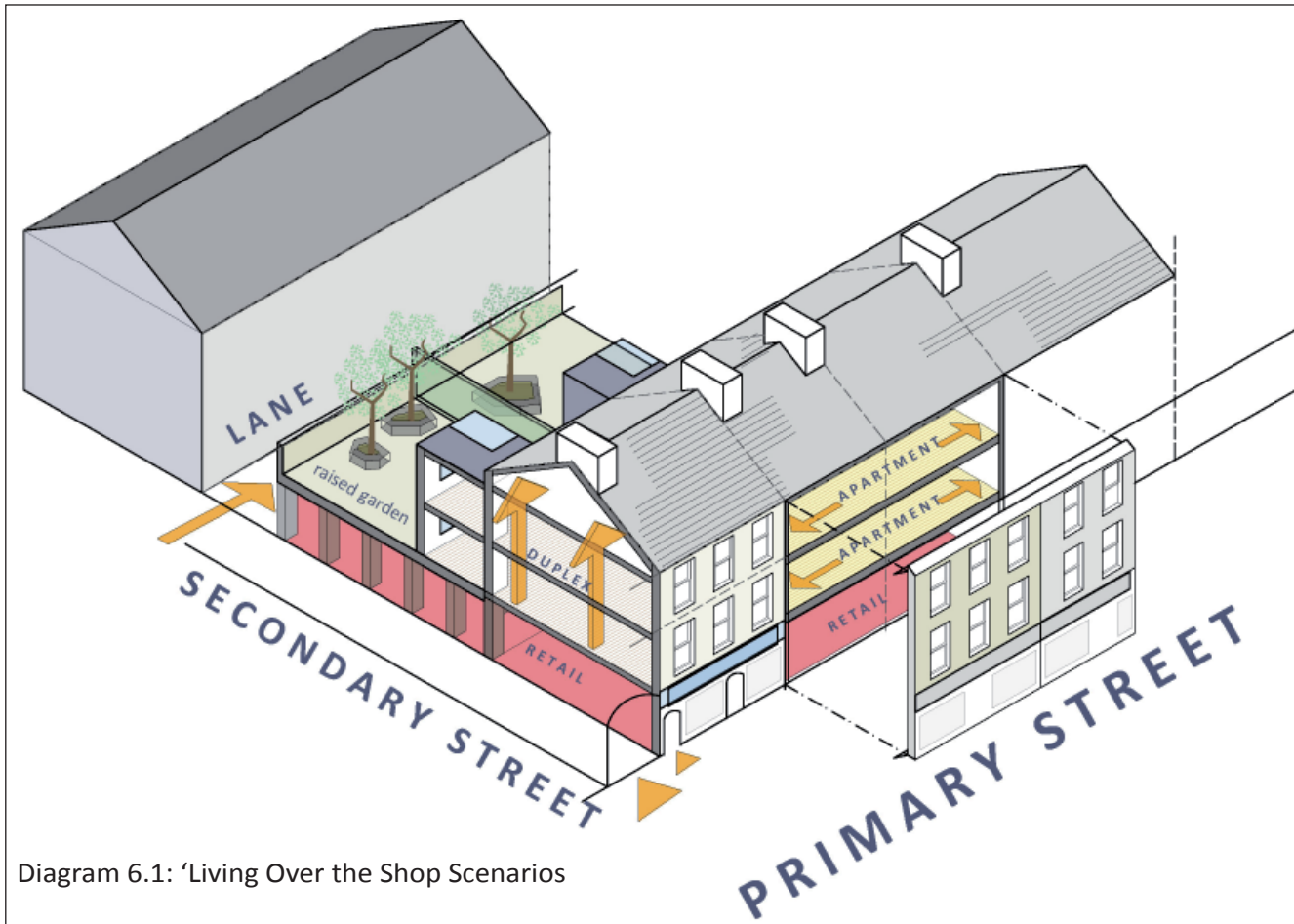


Diagram 6.1: 'Living Over the Shop Scenarios'

If people are to be attracted to live in town centres, the quality of residential space should be high. Rather than squeeze several residential units into a single building, it is advised that a smaller number of larger residences be created. Ideally, the upper floors above the shop would accommodate a single residence over 2, possibly 3 floors, depending on the height of the existing building. This can make a very attractive home and will minimise the interventions and loss of space due to fire lobbies, etc. Generally the existing buildings are too small for multi-unit sub-division, if quality of residence is the main objective.

This study suggests a rear extension at ground level to accommodate extended retail area/storage for the ground floor shop (or office, cafe, etc). This can then support a roof level garden overhead, providing external amenity space for the upper residential area. The diagram also shows a potential return to the rear of the upper floors, providing extended residential space where necessary.

Diagram 6.1: illustrates approaches to adapting and extending existing street buildings to accommodate residential units above ground floor retail/commercial units. This diagram shows 2 approaches: firstly a single residential unit occupying all the upper floors above the ground floor shop; secondly single level, upper floor, residential units which span across 2 buildings. The first option can provide a good quality dwelling with minimum interventions to address fire safety, acoustic separation, etc. The second option should only be considered where it can be achieved without adversely affecting important interiors and interior detailing. Localised openings between party walls is preferable to wholesale removal (which should be avoided, in particular where historic interiors and plan form survives), in order to maintain character of historic building plots. Only in exceptional circumstances should such amalgamation of protected structures be permitted, e.g. where little or nothing of historic merit survives internally or where the design and residential amenity proposed is of exceptional quality.

The diagram also shows the possibility of running retail and residential across 2 buildings by interconnection. This may present level difficulties as noted previously, however it provides another way of producing good quality living accommodation.

To encourage living over the shop as suggested in these diagrams, incentives such as subsidised residential parking permits, reduced development charges, review of development plan standards where high quality design and specification is proposed, should all be considered.

2. Existing Housing Stock

Drogheda possesses a rich variety of house types within the Heritage Quarter. The town remains a 'lived' in place. This is extremely important in the context of future change and development and the existing fine housing makes a critical contribution to this. Not all existing housing is currently used or presented to its best potential, however and development should seek to improve this.

Existing housing stock in Drogheda falls into 3 main types and should meet the following guidance:

1. existing housing in single dwelling use: This reflects the vast majority of housing within the Heritage Quarter and is generally in good condition reflecting a settled community. Encourage continued use as single dwellings. Where proposed to adapt to apartments, this will generally be undesirable and only considered where proposed adaptation will ensure quality residential units are retained and architectural integrity of interiors, plan form and gardens/external areas is not adversely altered.
2. Former houses which have been compartmentalised for multi-unit apartments. Encourage restoration as single residences or larger apartment units.
3. Empty/under-used housing (e.g. St. John's housing at Peter's Hill). Encourage reuse as single dwellings.



Map 6.1 Magdalene Street/William Street/Church Lane/King Street housing typologies



Fig. 6.26 Church Lane housing



Fig. 6.27 Church Lane housing



Fig. 6.22 St. John's at Peter's Hill



Fig. 6.23 Courtyard to St. John's



Fig. 6.24 & 6.25 Housing along William Street. Diverse typologies providing settled and quality homes within the town centre. It is important these remain as settled homes



Fig. 6.28 19th century houses off William Street, have been divided into small units. These houses would suit refurbishment as single homes (original function) or as large apartments (2no per house)

2. Unused Former Industrial

Drogheda has several former industrial buildings of varying scale which are currently vacant or under-used and could be successfully adapted for residential use, amongst other suitable uses. It may be appropriate in some instances to incorporate a mix of uses and opportunities exist in some places for new build development to take place in conjunction with the refurbishment and adaptation of the historic industrial structure.

While Drogheda has a number of converted warehouses, notably within the northern docks area, these tend to be smaller sized residential units with limited residential amenities.

Where conversion of industrial buildings is to take place the approach should aim to retain the industrial character of the buildings. Due to the often small sized window openings and low ceiling heights, open space apartments which extend across a number of window bays often make for successful interior layouts. Contemporary insertions to provide facilities such as kitchens and bathrooms can also work well.

As with all developments, quality materials, detailing and construction should be used to ensure durability. Compliance with Part L of the Building Regulations for former industrial type structures which are not protected can be challenging and care should be taken to avoid solutions which are not compatible with the construction typology (often solid wall, breathable masonry structures)

The examples illustrated to the right have all been developed by Urban Splash and show how high quality living accommodation can be provided in industrial buildings, while maintaining character.



Fig. 6.29 Albert Mill, Manchester, before conversion (source: Urban Splash)



Fig. 6.30 Albert Mill, Manchester, after conversion (source: Urban Splash)



Fig. 6.31 Albert Mill, Manchester, interior before conversion (source: Urban Splash)



Fig. 6.32 Albert Mill, Manchester, interior apartment, after conversion (source: Urban Splash)



Fig. 6.33 Albert Mill, Manchester, interior apartment, after conversion (source: Urban Splash)



Fig. 6.34 Former Brewery, UK, interior apartment, after conversion (source: Urban Splash)

6.4.2 New Residential Developments:

The opportunities for enhancing and increasing the new housing stock within the town centre are:

1. within infill sites, stitching into and repairing derelict elements of existing urban blocks
2. on under-utilised back lands behind main streets, creating low scale intimate, mixed-use environments, which might be accessed from existing or extended lane networks;
3. large, assembled development sites with the potential for the comprehensive development of new mixed-use urban quarters

6.4.3 Residential Development Standards:

The Drogheda Borough Council Development Plan 2011-2017, in chapter 6 on Housing and Community issues outlines the permitted spatial development standards, both internal and external, for new residential developments within the town. Certain relaxations of some of these standards are, within the plan, considered appropriate for residential developments within the town. Whilst it would not be desirable to relax these standards with regard to internal space standards, it may be considered that a certain quantum of stipulated public and private open space may be reduced where it can be demonstrated that alternative amenities and facilities, which might compensate for the lack of such space within the curtilage of the development, are available within the neighbourhood – as long as the requisite requirements for privacy are not compromised.



1. Infill Sites



2. Backlands Sites



3. Large Development Sites

Relaxation of development standards in the following respects might incentivise certain types of residential development within the town centre:

- Private open space requirements for houses within the town centre reduced to match those for comparably sized apartments or duplexes;
- Public open space requirements were exempted in the vicinity (5 minute walking distance), of the river, or an accessible park or green;
- Car-parking requirements to be met by the provision of a parking permit within the town parking facilities.

At the same time, other constraints, which would reinforce the unique historic character of the town should be consolidated and reinforced – appropriate building scale and massing, materiality, urban grain, permeability and connectivity within and around residential developments are all important considerations the proper application of which would ensure that new developments would compliment their surroundings.

6.5 Scale of Development

Scale and Drogheda

Significant changes in scale be it resulting from a large extension (vertical or horizontal) to an existing building or, a large-scale new building/complex, can have considerable impact on the prevailing physical character of any town. Generally scale is considered through a building's footprint, height and mass (three-dimensional form). Irish towns such as Drogheda display a diversity of scale which reflects ownership, function, civic importance, location and construction technique. An added factor influencing the historic scale is the way in which development tended to occur on a plot by plot basis, with developed plots combining to form streets and with the historic plot dimensions typically being narrow and deep.

Historically, the larger scale buildings within the Heritage Quarter of Drogheda tend to be the public, industrial and institutional buildings. In recent times some of the finer grain plots have been assembled and redeveloped as larger footprint buildings, typically for retail, commercial and residential (apartment) use with associated multi-storey parking. These buildings tend to be 2 or 3 storeys higher than the prevailing 3 to 4 storeys. Despite these more recent developments the nineteenth century scale of the town persists as the dominant character.

The town's topography imposes another strong influence on how the scale of buildings impact visually, in particular when viewed at a distance. The strategic vistas and views identified in Map 5.8 acknowledge the importance of the town's profile in giving distinctive character to Drogheda. This profile, formed by the sloping topography, is

sensitive to change and therefore the impact of large scale development on this profile must be considered when designing and assessing proposals for new development within the Heritage Quarter. While the town can absorb carefully considered localised variation in height, which maintains a vertical emphasis of form, where increased height extends across a number of historic plots, this can disrupt the verticality of profile and create a more monolithic wall-like horizontality which can jar with the prevailing profile.

Current policy on large scale building in Ireland

Current policy and guidance (e.g. Best Practice Urban Design Manual, Manual for Urban Streets and Road, DoAHG Guidelines for Architectural Heritage Protection and, Retail Design Manual) recommend retaining the fine grain pattern of historic towns. This is not only to protect historic character, but also reflects current urban design theory which recognises the contribution of frequency of plot to the making of active and vibrant streets and places.

More than tall buildings, the large scale building has been much more prevalent in recent years in Irish towns. These can have significant negative impacts on their urban environments, particularly historic urban environments. Typical negative impacts that must be avoided include disruption to prevailing/historic grain; excessive bulk; erosion of character of the area; inactive street frontage and loss of permeability; displacement; negative economic impact on existing commercial; deterioration of public realm due to traffic, shadow, wind, noise and other environmental impacts.

The large scale building, however, can have its place in the historic towns and the debate revolves around issues which include the importance to a

town's economic and commercial success of allowing the kind of uses which demand large footprint, large scale buildings; the desire to keep certain functions within the town centre to maintain diversity of function and civic, cultural and commercial status of the town centre, against the negative impact such developments can have on a town's distinctive character. It is important therefore to identify what purposes and where large scale buildings would be appropriate and to then set criteria/ standards which such development should meet to avoid negative impacts such as those identified above. Such criteria should always encourage contextually adapted design solutions which compliment prevailing character and which bring positive benefits to the existing fabric and public realm.

Large scale buildings may be identified as a potential building typology for specific purposes such as major regeneration, employment or shopping areas, or for a particular public/civic function such as a landmark civic building, theatre, museum or hospital. Drogheda's town centre has absorbed a number of large scale buildings in recent years which have provided a substantial amount of large footprint space for primarily retail and also residential and commercial use. At the present and for the mid-term future (5+ years) it is questionable whether Drogheda requires further large scale retail in the town centre and planning strategies should focus on ensuring that existing space is occupied before considering additional similar development. However, there are functions which Drogheda either lacks or current facilities are insufficient. For example the current borough council offices require upgrading and additional space to accommodate its functions and deliver the public services required.

Other examples might be the courthouse, cinema (permission has been granted for two cinemas within the Heritage Quarter) and certain types of office and residential uses.

Before large-scale buildings are to be considered a number of key questions need to be satisfied including 'is this the right location for such a building(s)' and 'could similar objectives be achieved with a series of smaller scale buildings'. A smaller scale building typology may be more suitable for an Irish urban context, particularly historic contexts, such as Drogheda.

Should a large-scale building be deemed necessary (and the appropriate rationale for its proposal is justified), the following considerations must also be addressed:

- Context – topography, morphology, genius loci
- Effect on historic environment, including built, cultural heritage, open space, views and vistas (visual impact)
- Effect on local environment, including micro-climate, flooding, biodiversity
- Local, district or town wide permeability and legibility benefits
- Public realm and civic design benefits
- Economic contribution and impact, including on existing uses within the town
- Function, carrying capacity and use intensity
- Transport and movement – most of the urban settings in Ireland do not currently support quality public transport, thus the impact of access by private vehicles must be addressed
- Potential cumulative impacts



Fig. 6.35 Large scale recent building



Fig. 6.36 Large scale mid-twentieth century building



Fig. 6.37 Recent large scale building onto River Boyne.



Fig. 6.38: early twentieth century view of Drogheda showing a range of building scales, dominated by slender spires



Fig. 6.39 early twentieth century view of Drogheda showing a range of building scales, dominated by slender church spire and Boyne Viaduct in background



Fig. 6.40 The spire of St. Peter's provides a dominant visual presence along West Street.

6.5.1 Scale Guidance for Drogheda's Heritage Quarter

In determining the appropriate scale of new development, or extensions to existing buildings, in the Heritage Quarter, the following considerations will apply:

1. Consideration of Character Areas (Maps 5.8 and 6.3): How does proposed development address, enhance and reinforce the prevailing character area in which it is located? Of particular note is the objective to maintain the distinctive height contrast between the areas Within the Walls - Town Centre and the adjacent Settled Residential Areas. By doing so, the line of the Medieval Walls can be appreciated and understood, thus supporting the objectives and policies of the Town Walls Conservation Plan.

2. Architectural Conservation Areas and Protected Structures (Maps 5.2 and 6.4): The Town Development Plan and the DAHG Guidelines on Protection of Architectural Heritage set out objectives to protect the character of protected structures and ACA's. Development within these areas must take into consideration impact on these contexts.

3. Primary Urban Spatial Sequences (Map 6.5): These have already been identified in Chapter 2 as being of national importance due to their urban quality. The spatial quality can be disrupted by development being too large and too small in scale. Therefore new development should conform to the existing prevailing scale along these streets.

4. Views and Vistas (Maps 5.9 and 6.6): New development shall have regard to the strategic

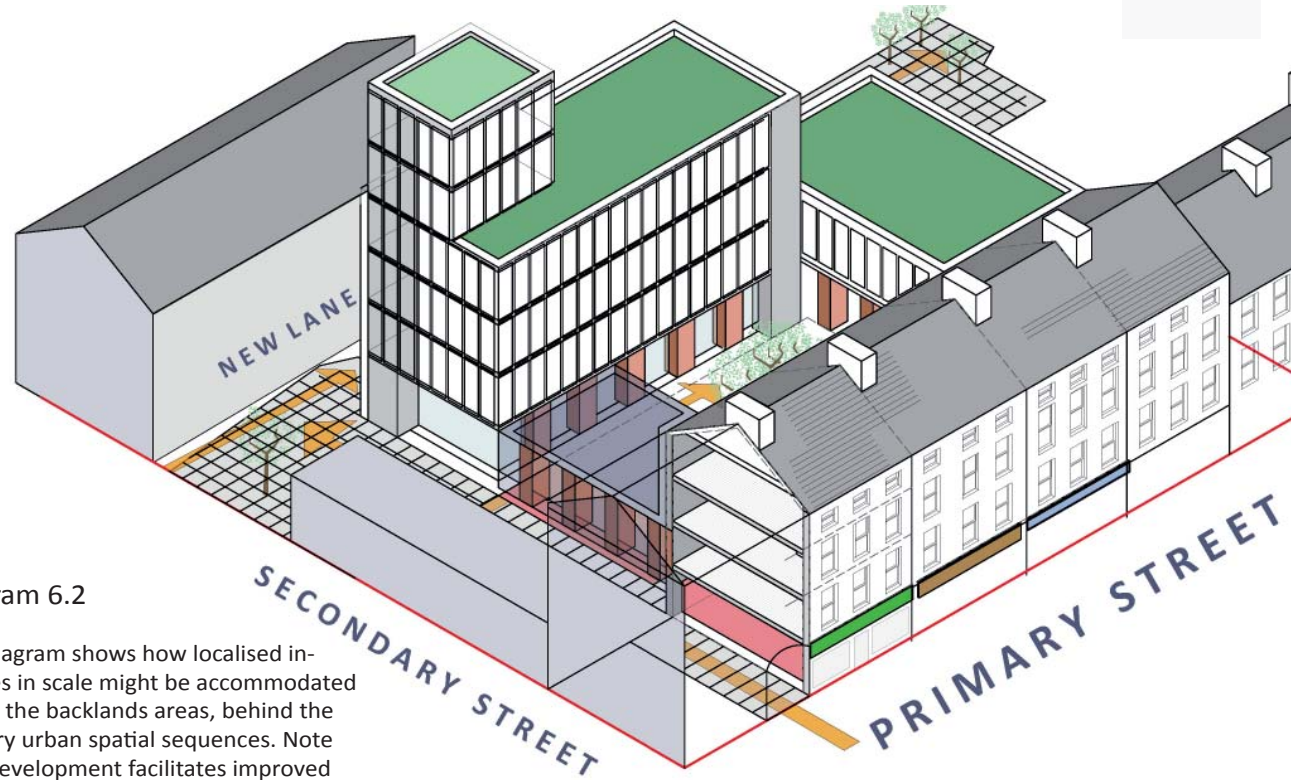


Diagram 6.2

This diagram shows how localised increases in scale might be accommodated within the backlands areas, behind the primary urban spatial sequences. Note how development facilitates improved public realm/permeability

views and vistas identified in aforementioned maps. Views to and from landmarks which relate to the strategic views should not be blocked or negatively affected by new development. Development should not adversely alter the character of strategic vistas.

5 Topography: Sections 6.7, 6.8 and 6.9 show the existing cross section, south-north, through three sample locations within the Heritage Quarter. These are provided by way of example only, and are useful in illustrating in an abstract manner, the topography of Drogheda and existing building scale and profile.

These cross sections can be used to assess the potential impacts - positive and negative - of new

development proposals, as shown in sections 6.9 and 6.10. These sections use the design scenarios which are described in section 6.7 of the Framework Plan. They show how larger scale development might be accommodated without adversely affecting the prevailing qualities and character this Framework Plan aims to protect and enhance. This approach, to insert development proposals into the larger urban cross section, can provide a useful planning tool to assess impact and should be a requirement for all new development in the Heritage Quarter. A longer term objective, however, should be to create a 3-D digital model of the Heritage Quarter which would then be used to assess proposals more accurately.

6. Performance Criteria for large scale development in the Heritage Quarter: The objective of the performance criteria set out in this section is to ensure any proposed development of large-scale building will enhance the quality of the human environment – for such significant development it is not sufficient to merely avoid damaging the existing environment.

The intention of the performance criteria is to be dynamic, interactive and enabling of a holistic process. They should not be seen as a ‘tick-the-box’ exercise, where the only concern is ‘if’ the criteria have been taken into account. The objective is to stimulate critical thinking and responsive design.

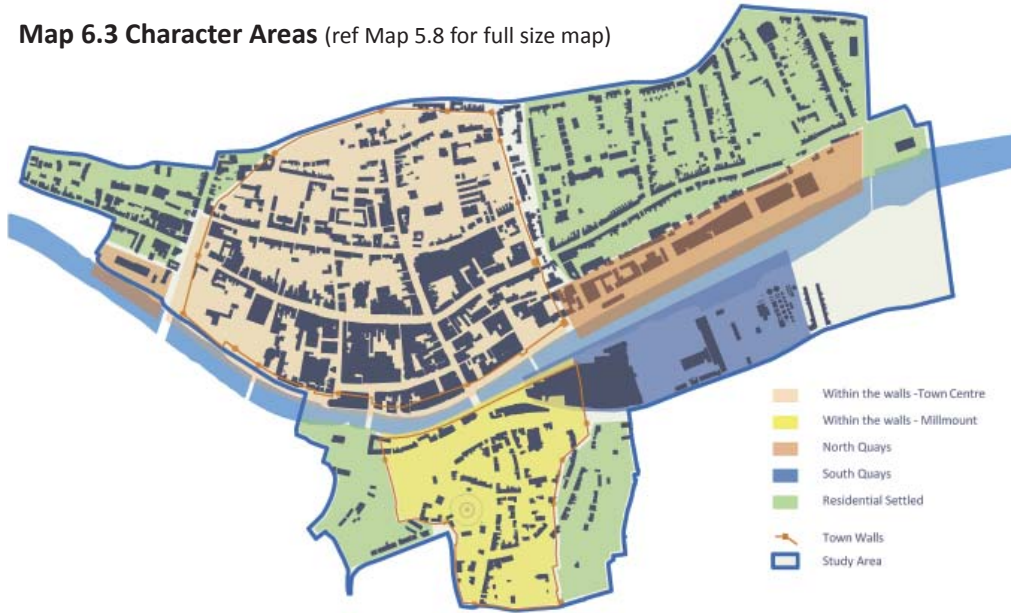
This dynamic approach is also important as not all criteria may be relevant in the case of every large scale building proposed. The approach taken will depend on scale of proposed development and sensitivity of the urban context.

The criteria are listed here as headings and large scale development proposals should be required to show how these have been taken into account in the design and scheme proposals and how the cumulative impacts are addressed.

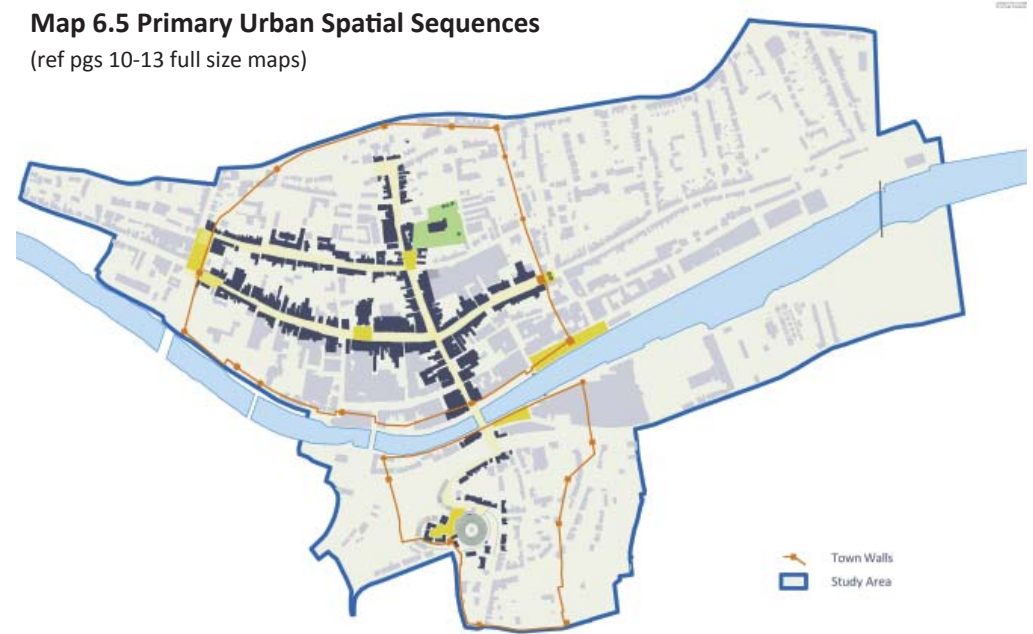
CRITERIA	CONSIDERATIONS
CONTEXT	<ul style="list-style-type: none"> • Topography • Morphology & Grain • Streetscape • Skyline • Scale Relationship • Height • Visual Impacts - views and vistas • Character/Sense of Place
EFFECT ON HISTORIC ENVIRONMENT	<ul style="list-style-type: none"> • Historic Urban Landscape - Character/Sense of Place • Built heritage -integration, renewal of existing buildings of architectural heritage status • Open Space
EFFECT ON LOCAL ENVIRONMENT	<ul style="list-style-type: none"> • Micro-climate • Wind • Sunlight & Daylight • Shadowing • Noise - from use; mechanical plant, traffic generated • Night time appearance • Illumination • Reflection/glare • Flooding • Water & Waste: macro/micro
DESIGN & CONSTRUCTION QUALITY	<ul style="list-style-type: none"> • Form • Massing • Proportion • Silhouette • Materials & Construction • Systems • Durability • Design Excellence • Roof profile/top of building • Signage/Branding

CRITERIA	CONSIDERATIONS
PERMEABILITY & LEGIBILITY	<ul style="list-style-type: none"> • Site Connections - disruption of existing lanes/ opening up new lanes/ links • Wider Area Connections • Wayfinding
TRANSPORT & MOVEMENT INFRASTRUCTURE	<ul style="list-style-type: none"> • Existing Infrastructure & Capacity • Feasibility of Improvement • Public transport proximity and links • Pedestrian links • Cyclists links • Quality of links
SUSTAINABILITY	<ul style="list-style-type: none"> • Reason for being (Raison d'être) • Energy Performance • Community/Stakeholder Input • Social/Public/Cultural Contribution (Planning Gain) • Use & Use Mix • Residential Amenity • Adaptability • Credibility/Deliverability • Economic Contribution
CUMULATIVE IMPACTS	

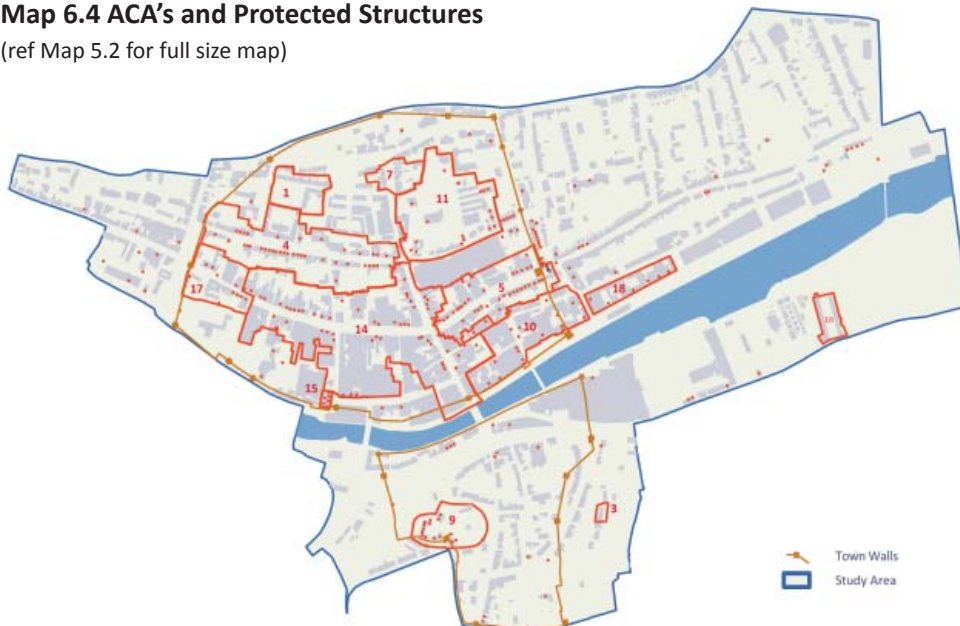
Map 6.3 Character Areas (ref Map 5.8 for full size map)



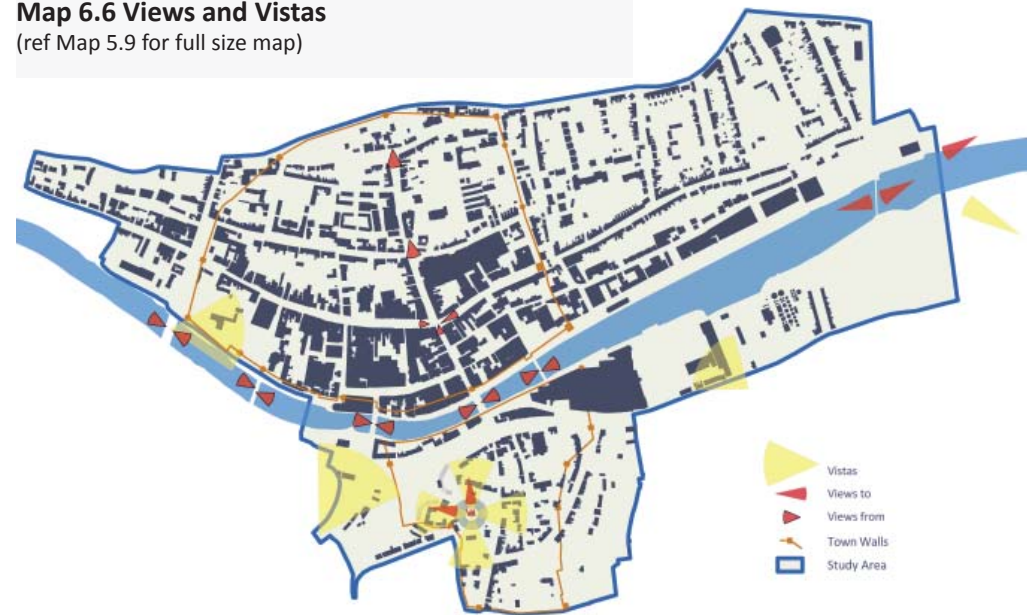
Map 6.5 Primary Urban Spatial Sequences
(ref pgs 10-13 full size maps)

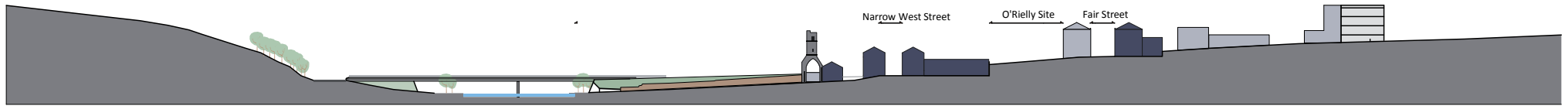


Map 6.4 ACA's and Protected Structures
(ref Map 5.2 for full size map)



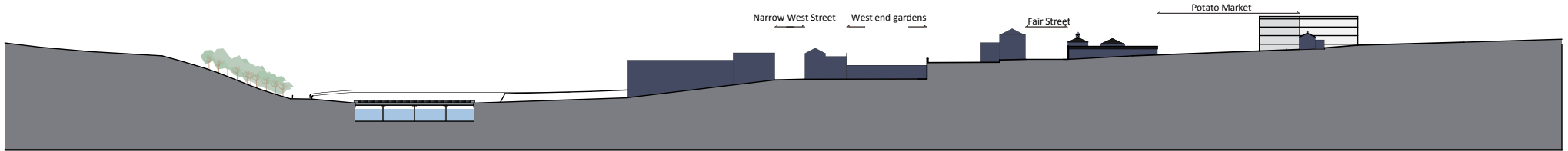
Map 6.6 Views and Vistas
(ref Map 5.9 for full size map)





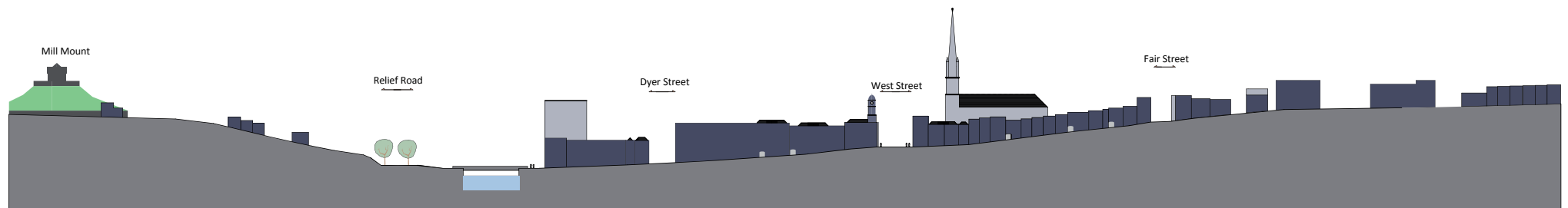
site section 1-1 existing

Section 6.7: Existing section through Abbey Lands / Narrow West Street / Fair Street/ Bolton Street / Patrick Street



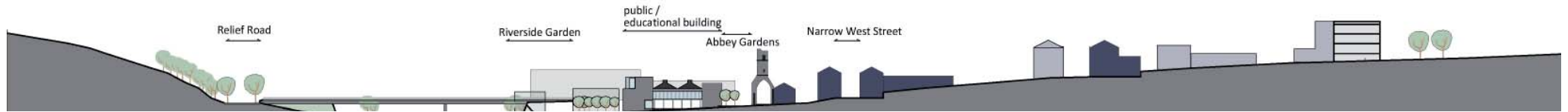
site section 2-2 existing

Section 6.8: Existing section through Abbey Centre, Narrow West St, West End Gardens, Fair Street, Cornmarket, Potato Market

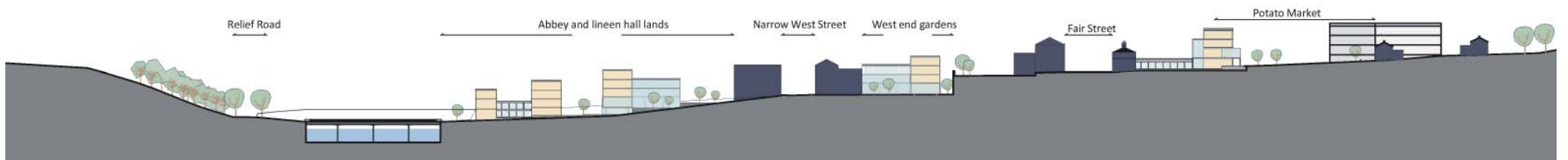


site section 3-3 existing

Section 6.9: Existing section through Millmount, River Boyne / St Mary's Bridge / Shop Street, /St. Peter's Street, /Magdalene Street



Section 6.10: Schematic section through Abbey Lands / Narrow West Street / Fair Street/ Bolton Street / Patrick Street using the scenario proposal described in section 6.7



Section 6.11: Schematic section through Abbey Centre / Narrow West St / West End Gardens / Fair Street / Drogheda Borough Council Offices / Potato Market / Patrick Street showing scenario proposal described in section 6.7.

In developing the scale strategy for Drogheda, Strategic Objective 4 of the DBC Development Plan 2011-2017 is worth noting - *“Enhance the established role of Drogheda Town Centre as the retail, social and transport focus of the entire Borough and pro-actively position the town centre as a destination of choice for both visitors and residents alike with a special emphasis on the role played by the town walls”*.

There is a notable clarity between the built form within the town walls and without (with the exception of the Port area and the mill complex west of the Peace Bridge). The endurance of the historic significance of the walls is manifest in this distinction and retaining this distinction will go a long way towards achieving the special emphasis proposed in the Strategic Objective 4. Thus, any height policy aims to ensure a clear physical definition between within and without the walls. This policy, however, does not override consideration of any proposed development’s context especially along the Primary Urban Spatial Sequences, within ACA’s and schemes affecting the setting of protected structures.

6.6 Public Realm

This section provides some general guidance for public realm.

There are five primary components to the defining attributes of Drogheda's urban structure. It is useful to consider these within the context of establishing both macro-level policies for the public realm and to identify strategic sites where public realm improvement projects might be prioritised.

The five components are:

1. The primary urban spatial sequences identified in section 2.0
2. The River Boyne, encompassing the quays, edges, bridges, etc which provide direct/immediate access
3. The lanes - in particular the urban lanes
4. The Town Walls - while only partly surviving as upstanding walls, they are real and acknowledge defining elements of Drogheda's physical character. Already identified in the height and mass strategy, the walls continue to exert and influence on the prevailing scale
5. The Topography

These are presented in map and diagram form on the following pages.

In terms of the macro-level approach, this proposes that each of the five primary components be planned, managed and designed in a strategic way, such that individual development projects, where relevant, are considered in terms of their impacts - positive and negative - at the macro scale. Opportunities for individual developments to contribute to, for example, the delivery of improved Riverside access, should be taken. Further, however, it may be

appropriate that such contributions conform to a consistent design and material approach, in order to achieve overall coherence. This approach must always be managed to allow for site specific circumstances, for example the retention of historic kerbs or other elements of street furniture which might survive and be desirable to retain and integrate within the overall design approach.

These five components of Drogheda's urban structure are worthy of highlighting due to their distinctive qualities which contribute to the very particular character of the town. In short, apart from the stand out landmarks, these are the memorable elements of Drogheda. So it is proposed that their improvement and enhancement is considered in a unified way when tackling the public realm.

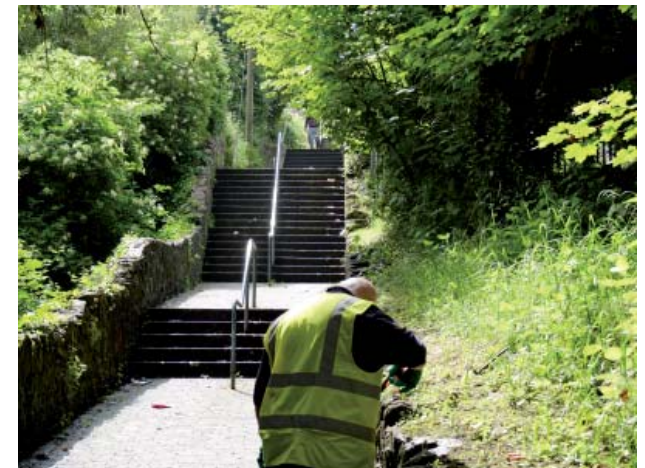
The scenarios schematics in section 6.7 explore a number of potential public realm improvement projects which would form part of this strategic approach and represents important initiatives which might be implemented early.



6.41: 'Abbey Lane': A most distinctive and unusual element of Drogheda's public realm is the lane which has been formed out of the ruinous Abbey church, running east-west through the former chancel and nave. This idiosyncratic feature deserves to be better presented, maintained and integrated. The architectural and archaeological significance of the monument needs to be preserved within the context of improved urban integration and information should be provided



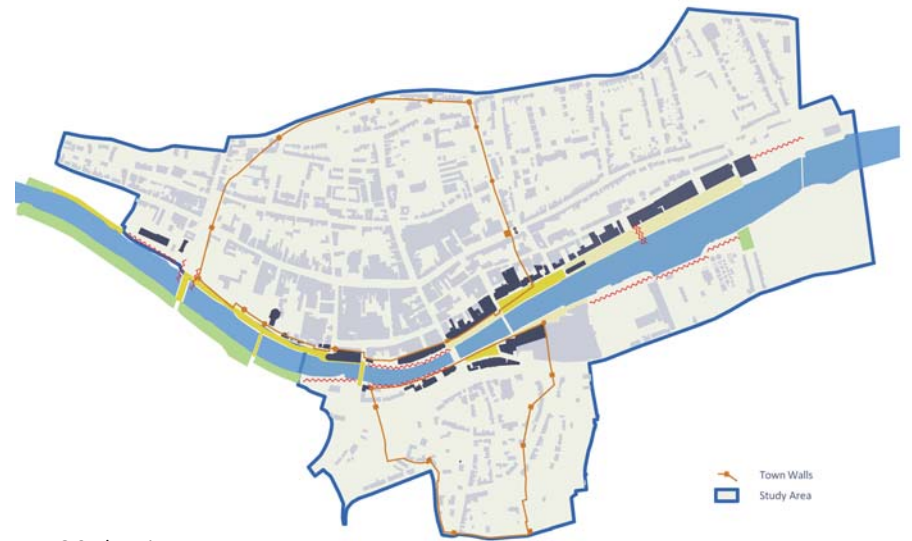
6.42 St. Peter's Hill: important civic space within principal urban sequence



6.43 Steps from Millmount to Donore Road - an important link and green route. Well maintained and contributes to green infrastructure of the Heritage Quarter



Map 6.7 Primary Urban Spatial Sequences



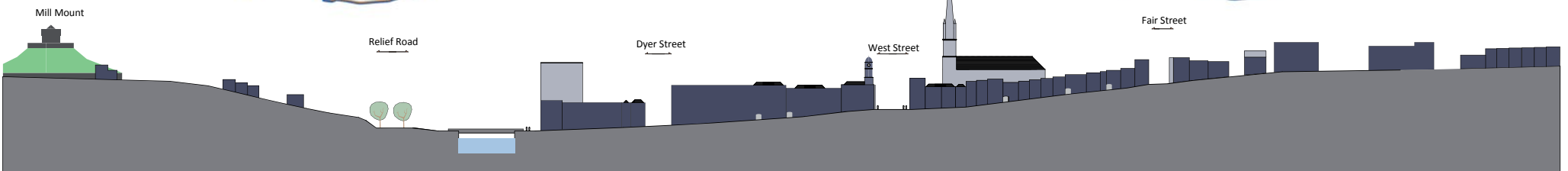
Map 6.9 The River



Map 6.8 The Lanes



Map 6.10 The Town Walls



The Topography

6.6.1 Principal Recommendations arising from analysis of Urban (Paved) Spaces

1 & 2. Millmount Spaces

Ambience: Improve heritage setting; views and information

Activity: Improve street furniture, seating and social uses

Movement: Improve pedestrian and cycle mobility, provide vehicular drop off and cycle racks

Nature: Improve biodiversity

3. Dublin Gate

Ambience: Improve heritage setting; views and information. Microclimate potential

Activity: Improve street furniture, seating and social uses

Movement: Improve pedestrian and cycle mobility, provide vehicular drop off and cycle racks

Nature: Improve biodiversity

4. Bull Ring & 5. South Quay

Ambience: Improve heritage setting; views and information

Activity: Improve street furniture, seating and social uses

Movement: Improve pedestrian and cycle mobility, especially across main road, provide cycle racks

Nature: Improve biodiversity

6. Donore Road Junction

Ambience: Improve heritage setting; views and information, especially Buttergate and bridge crossing

Activity: Improve street furniture, seating and social uses

Movement: Improve pedestrian and cycle mobility, especially across main road, provide cycle racks

Nature: Improve biodiversity

Map 6.11 Paved Urban Spaces
(Ref Map 5.3 for full size)



9. St. Dominick's/Shop Street

Activity: Improve street furniture, seating and social uses

Movement: Improve pedestrian accessibility

10. The Mall (see scenario schematic at 6.7)

Events: Improve event potential

Ambience: Improve heritage setting; views and information. Microclimate potential

Activity: Improve street furniture, seating and social uses

Movement: Improve pedestrian and cycle mobility, especially crossing from bridge, provide vehicular drop off and cycle racks

Traffic calm, and reduce parking

Nature: Improve biodiversity

Note potential building development opportunity site (existing buildings)

Fig. 6.44: The Mall in early 20th century (source: www.nli.ie)





Fig. 6.45: St. Peter's Hill



Fig. 6.47: Millmount



Fig. 6.48: St. Peter's Piazza



Fig. 6.46: Cornmarket/Fair Street - Fair Street

11. St Lawrence's Gate:

Improve night time view
Review signage and visual elements

14. West Gate

Ambience: Improve heritage setting; views and information
Activity: Improve street furniture, seating and social uses
Movement: Improve pedestrian and cycle mobility, especially across main road, provide cycle racks
Nature: Improve biodiversity

16. Potato Market

Events: Improve event potential
Ambience: Improve heritage setting; views and information. Microclimate potential
Activity: Improve street furniture, seating and social uses.
Movement: Improve pedestrian and cycle mobility, provide vehicular drop off and cycle racks
Traffic calm, and reduce impact of parking (create multi uses)
Nature: Improve biodiversity

17. Peter's Hill Square (see scenario schematic proposal at section 6.7)

Ambience: Improve heritage setting; views and information. Microclimate potential
Activity: Improve street furniture, seating and social uses
Movement: Improve pedestrian and cycle mobility, provide vehicular drop off and cycle racks
Traffic calm, and reduce impact of parking (create multi uses)
Nature: Improve biodiversity

23/ Bolton Gate Space (see sketch design at section 7.3)

Ambience: Improve heritage setting; views and information
Activity: Improve street furniture, seating and social uses
Improve event potential and use of space
Movement: Improve pedestrian and cycle mobility, provide vehicular drop off and cycle racks
Nature: Improve biodiversity
Note potential building development opportunity site (restoring streetscape on Rope Walk)

Principal Recommendations arising from analysis of Green Open Spaces (Ref Map 5.4)

1. St Mary's Cemetery

Ambience: Improve heritage setting; views and information

Movement: Improve pedestrian mobility, provide vehicular drop off and cycle racks nearby

2. The Dale

Ambience: Improve heritage setting; views and information

Activity: Improve street furniture, seating and social/recreational uses

Movement: Improve pedestrian mobility and cycle, provide vehicular drop off and cycle racks nearby

Nature: Improve and manage biodiversity (note long lived trees are scarce)

3. Buttergate

Ambience: Improve heritage setting; views and information

5. North Strand Esplanade

Movement: Improve pedestrian and cycle accessibility, provide vehicular drop off and cycle racks nearby

Activity: Improve street furniture, seating and social/recreational uses

Note potential building development opportunity site (recreation uses)

6. Cord Burial Ground

Ambience: Improve heritage setting; views and information

Activity: Improve street furniture, seating and social/recreational uses

Movement: Improve pedestrian accessibility, provide vehicular drop off and cycle racks nearby

Nature: Improve and manage biodiversity



Fig. 6.49: Buttergate



Fig. 6.50: The Dale



Fig. 6.51: Front Gardens

7. Sienna Convent and Cemetery (note this is semi private space)

Ambience: Improve heritage setting; views and information

Activity: Improve street furniture, seating and social/recreational uses

Movement: Improve pedestrian accessibility, provide vehicular drop off and cycle racks nearby

Nature: Improve and manage biodiversity

8. St. Peter's Graveyard

Ambience: Improve heritage setting; views and information

Movement: Improve pedestrian accessibility

9. Magdalene St /Rope Walk/Bolton Gate Space (see scenario schematic proposal at section 6.7)

12. Front Gardens

Encourage front gardens as part of heritage of town and instigate front garden festival

13. Private Gardens

Consider garden open day/event



Fig. 6.52: : St. Peter's Graveyard.

Principal Recommendations arising from analysis of River Corridor Spaces

1. West of Peace (Boyne Road) Bridge

Events: Create places for events associated with the river

Ambience: Improve setting and views, capitalise on microclimate, especially of south facing bank, and provide information

Activity: Improve interaction with river, and recreation/play opportunities, seating, furniture, hangout and social spaces.

Movement: Improve pedestrian and cycle access, and public transport access.

Nature: Build on existing biodiversity

2. Peace Bridge to Haymarket Bridge

Events: Create places for events associated with the river

Ambience: Improve setting and views, capitalise on microclimate, especially of south facing bank, and provide information

Activity: Improve interaction with river, and recreation/play opportunities, seating, furniture, hangout and social spaces.

Movement: Improve pedestrian and cycle access, and public transport access, and access to river.

Nature: Improve biodiversity, trees and planting

3. Haymarket Bridge to St. Mary's (Bullring) Bridge

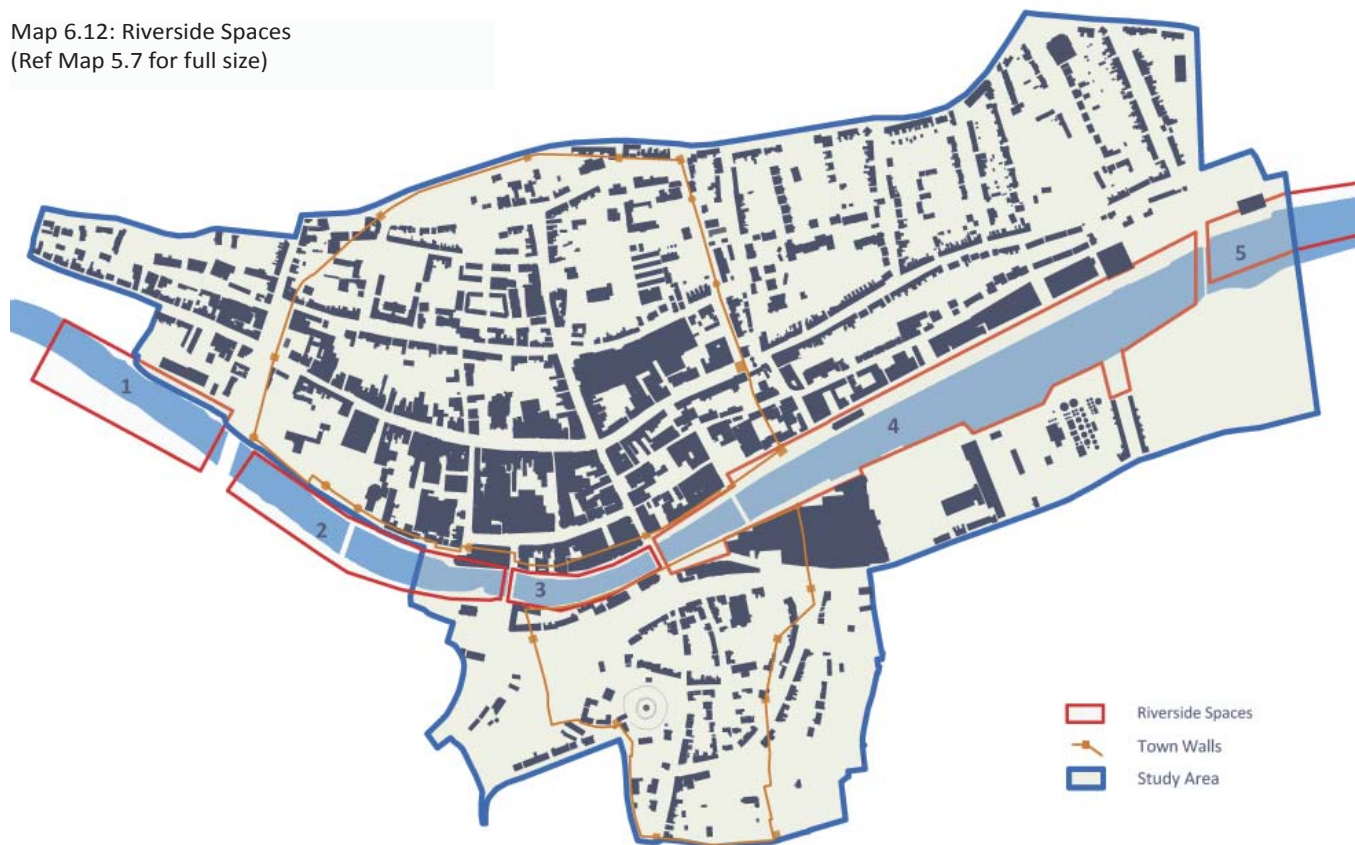
Ambience: Improve setting and views, capitalise on microclimate, especially of south facing bank, and provide information

Activity: Improve interaction with river, and recreation/play opportunities, seating, and furniture.

Movement: Improve pedestrian and cycle access, and access to river.

Nature: Improve biodiversity.

Map 6.12: Riverside Spaces
(Ref Map 5.7 for full size)



4. St. Mary's (Bullring) Bridge to Boyne Viaduct:

Ambience: Provide Information, Improve setting and views, capitalise on microclimate, especially south facing bank

Activity: Improve interaction with river, and recreation/play opportunities, seating, furniture, hang-out and social spaces.

Movement: Improve pedestrian and cycle access, and public transport access, and access to river.

Nature: Improve biodiversity, and trees.

5. East of Boyne Viaduct

Ambience: Improve setting and views, capitalise on microclimate, especially south facing bank, and provide information

Activity: Improve interaction with river, and recreation/play opportunities, seating, and furniture.

Movement: Improve pedestrian and cycle access, and access to river.

Nature: Build on existing biodiversity.

6.6.2 Street Paving & Street Furniture Design Guidance

Introduction

The town of Drogheda as it was originally laid-out would, historically, have suited pedestrian movement. However, particularly as transportation and movement evolved throughout the 20th century, this has changed.

In order to resolve the conflicts between the diverse modes of transport and access, some re-assessment of the design and layout of the public realm needs to take place.

The ease at which this desirable re-balancing of traffic movements in favour of the pedestrian can take place is directly influenced by the design of public streets and spaces and the quality of surface materials chosen.

This policy document aims to ensure that development within Drogheda Heritage Quarter responds to an agreed standard to ensure a coherent approach across a range of land-ownerships resulting in a high quality, vibrant, robust and legible place to visit work and live.

Policy Approach

Coinciding with the completion of this report is the publication by, jointly, the Department of Transport, Tourism and Sport and the Department of Environment, Communities and Local Government, of the Design Manual for Urban Roads and Streets (DMURS). It is anticipated that this document will become the controlling policy document for the design/upgrade of all urban roads and streets and will inform all urban development. It contains a number of directives which are envisaged as being mandatory requirements for the design of the public realm, specifically relating to urban roads and streets.

Therefore, in offering policy or design guidance on paving and street furniture within this document, it is not intended to either repeat or over-ride the recommendations or guidance offered in the DMURS document

The motivating factors behind this new policy are consistent with many of those which inform this document in terms of the creation of sustainable places which will promote a broad range of societal issues, '*ranging from economic development, employment, tourism and recreation, through health, crime and security and onto education, social inclusion, energy efficiency and climate change*'. For example, Local authorities will be obliged to facilitate implementation of the principles, approaches and standards to road and street design set out in the DMURS in carrying out their development planning functions under the Planning Code. Also, City and County Development Plans will be required to reference this DMURS to facilitate the implementation of required policies for sustainable living. Local Area Plans and also other non-statutory plans such as this one should also reference the principles, approaches and standards within the DMURS, where appropriate.

The manual encourages Local Authorities to facilitate the adoption of the multi-disciplinary approach to consultation where appropriate and they will be required to use the relevant standards in the Manual when assessing planning applications which relate to or impact on urban roads and streets, whilst at the same time requiring applicants to adopt the design principles, approaches and design standards of the DMURS.

In the case of local authorities' own development in relation to design of urban roads, streets or networks, local authorities will be required to facilitate, as appropriate, the multi-disciplinary design teams advised in the DMURS and ensure the principles, approaches and standards outlined therein are applied as appropriate.

The four over-arching design principles outlined in the document are:

1. The creation and management of permeable and legible street networks.
2. The creation of self-regulating streets that cater for the various place and movement functions of a street.
3. To provide design standards for the creation of a safe, comfortable and attractive pedestrian environment.
4. The implementation of a more integrated approach to street design.

Historic Context

The DMURS recognizes that additional design considerations must be taken into account in areas of historic significance that are highly sensitive to interventions, stating that the most appropriate course of action should be to minimise any level of intervention to existing historical features.

It recommends that the street surface and any features set into it such as coalhole covers, weigh-bridges, pavement lights, cellar doors etc. Elements of street furniture associated with the historic use of the street should be identified and protected and that an 'assessment of significance' should be prepared when dealing with interventions within historic core areas.

For example when dealing with an established street layout and associated materials a distinction is drawn between three levels of significance:

1. Undisturbed areas of existing historic streets, which have the highest value and bear witness to the skill of historic craftsman;
2. Areas where streets have been altered or reconfigured using the original design/material;
3. Reinstated street areas re-using salvaged material from other places

In the development of Design Guidance documentation which specifically relates to the Heritage Quarter of Drogheda Borough Council, and in light of the recommendations in the DMURS manual for the establishment of interdisciplinary teams, reference to policy/guidance documents such as 'Paved with Gold' published by the Commission for Architecture and the Built Environment and 'Link & Place: A Guide to Street Planning and Design' which offer both quantitative and qualitative strategies for the assessment of areas of the public realm which address the concerns of Road, Traffic and Transportation engineers as well as Architects, Urban Designers and Landscape Architects will help to identify the positive qualities which exist and require reinforcement and enhancement.

Within the context of a historic town like Drogheda, these positive qualities are enhanced by maintaining respect for the distinctive historic elements of streetscape which help to give the town its unique character.

In developing strategies for sustainable development within the historic core of Drogheda, the establishment of multi-disciplinary teams to devise design solutions which address the inevitable conflicts and contradictions which exist amongst those who live in, work in and visit the town. The over-riding ambition should be the creation of a high quality environment for all appropriate activities.

Streets that are good quality places achieve a number of positive outcomes including:

- Attractive and well-connected permeable street networks encourage more people to walk and cycle to local destinations, improving their health while reducing motor traffic, energy use and pollution

- More people on the streets leads to improved personal security and road safety. Research shows that the presence of pedestrians on streets causes drivers to travel more slowly
- People meeting one another on a casual basis strengthens communities and encourages a sense of pride in local environments
- People who live in good quality environments are more likely to have a sense of ownership and a stake in maintaining the quality of their local streets and public spaces.

Streetscape Components

There are key design objectives that are common to all streets within the town, both in dealing with the refurbishment/upgrading of existing elements and the introduction of new elements. By looking at the street as a series of interrelated components, these common design objectives can be applied across any typical street.

Carriageways

The carriageway makes up a large visual component of the streetscape and therefore has a significant visual impact on the streetscape. Design needs to ensure that the visual simplicity of the carriageway is maintained by keeping different coloured surfaces to the minimum required for the effective demarcation and operation of bus and cycle lanes.

Kerbing

The design and quality of kerbs has a substantial effect on the streetscape and requires careful attention. The design needs to ensure that:

- Existing stone kerbs are to be maintained and refurbished as much as possible
- Where new kerbs are necessary they are to be of high quality and in character with the existing

- Kerb alignment is consistent and follows smooth flowing lines to provide strong definition between footway and carriageway. Kerb build-outs and the creation of services parking bays should relate to the area, building form and function
- Kerb lines should be formed with radius kerbs in preference to mitred kerbs
- Kerbs should be carefully detailed with crossing points considered.

Footpaths

Pedestrian access along footpaths should be facilitated by:

- Maximising pavement space where possible and when appropriate, and ensuring that, where footpaths which contain historic kerbs or paving stones that these materials are re-used and complimented with appropriate new materials
- Ensuring high quality surface with consistent joint alignments, no unsightly mortar infill and simple combinations of materials
- Ensuring high quality pavements and well drained surfaces, free of trip and slip hazards.
- Creating a legible hierarchy of paving materials that visually locates pedestrian, cycle and vehicle priority areas
- Continuing pavement surface finish material across vehicle crossovers
- Positioning of street furniture for the benefit of pedestrians and ensuring pedestrian movement is unobstructed so that pedestrians of all abilities can move along the street independently and unhindered.

Crossing Points

Where safety and traffic priorities permit, the design must ensure that:

- They are located with due regard for pedestrian desire lines and accessibility

- Adequate space is provided for the volumes of pedestrians and cyclists using the crossing
- The amount of pedestrian guardrails and other street furniture associated with the crossing is kept to the minimum required for safety.

Street Furniture

The over-riding philosophy, in terms of a policy regarding street furniture, should be to keep it to a minimum – to oblige developers to provide as many elements as is reasonably possible within the curtilage of their development sites. However, the public realm tends to become populated with elements by stealth – each utility company or public service provider inserting their own elements quite often without any reference to design quality, location, or coordination with other elements in the street.

The scope of any policy which might be adopted should cover Drogheda Borough Council's requirements for siting, frequency, appearance, materials, fixing of all street elements and should inform any management control process by which such a policy can be implemented and sustained.

In addition to such a control function, a street furniture policy should avail of the opportunity to 'brand' the town in a unique way

Well designed street furniture can add to the character of an area and in its own way become an iconic representation of a city or region – e.g. English post-boxes and telephone kiosks, Amsterdam bollards, Parisian advertising drums etc...

Approach to development of policy on street furniture

New street furniture should only be installed if absolutely necessary, with a new development, or as part of a traffic management improvement. It must be placed with a sensitivity to visual order. New

furniture shall be limited in style variation, must have a design that will age gracefully, and must be durable and robust. Ornamentation shall be limited and appropriate. In developing a policy for the design, specification, procurement or selection of street furniture, consideration should be given to the suitability of any particular recommendation in terms of its robustness, effectiveness, design quality, cost, siting and compatibility with other elements of furniture. The design guidance for street furniture needs to set out carefully policies in relation to the function of the receiving spaces and to avoid unnecessary clutter. The public realm should incorporate a range of furniture solutions that are integral to the space and, where suitable, encourage people to linger and socialise. The location of furniture in areas prone to anti social activity needs to be carefully considered. Within key areas, special bespoke designs should be promoted. Best practice precedents should be examined in terms of the processes involved in procuring them and the effectiveness of their end results. Section 254 licences shall have regard to the guidance in this Plan and ACA and protected structures policy.

Location of Street Furniture

When determining the principles of locating street furniture within the footway, including for Section 254 licences, where all elements of street furniture are located between the face of the buildings and carriageway, account must be taken of a number of interrelated factors:

- Available footway and verge widths
- Vehicle flows
- Pedestrian flows
- Parking and loading requirements
- Land uses adjacent to the public realm
- Regulations governing street furniture size and location requirements
- Security
- Special needs of pedestrians with a range of mobility impairments

Selection of Street Furniture

- Choose street furniture to relate to its location and local distinctiveness, and reinforce a sense of place
- Different items of street furniture should relate to each other in terms of design, siting and colour
- Avoid causing clutter
- Reduce to a minimum 'defensive' street furniture such as railings and bollards
- Retain and refurbish distinctive historic elements of street furniture.

Universal Design

Not all disability relates to difficulties with mobility. People with sensory or cognitive impairment are often less obviously disabled, so it is important to ensure that their needs are not overlooked. Legible design, i.e. design which makes it easier for people to work out where they are and where they are going is especially helpful to disabled people. Not only does it minimise the length of journeys by avoiding wrong turns, for some it may make journeys possible to accomplish in the first place.

Part of the guidance provided in Part M of the Building Regulations are best practice principles and may be adhered to in relation to the public realm to ensure that design and detailing of streets works for all users.

Guidance relating to the way in which vehicle routes are separated from pedestrian routes, tactile paving requirements, drainage considerations and the selection of surface materials are considered. Tactile paving shall be appropriate colour and material for historic setting. Another element detailed is in relation to acceptable gradients for users to approach buildings and also to use external spaces.

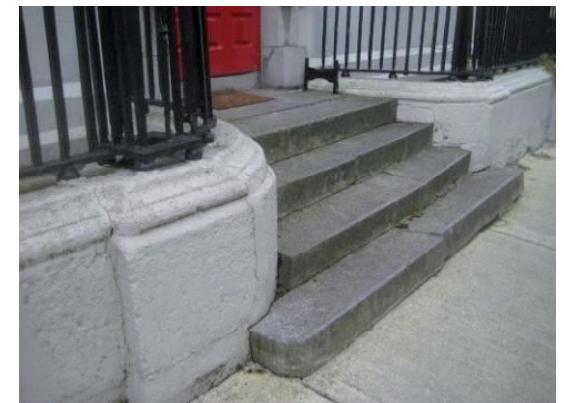
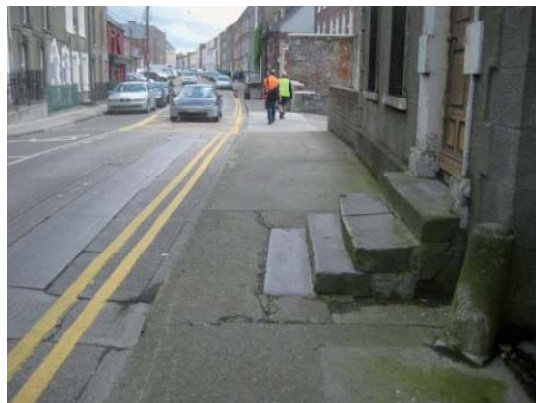


**Historic streetscape elements - coal hole covers, walls, steps, plinths
To be integrated within public paving/realm improvement
schemes. These examples are all taken from the Heritage Quarter.**

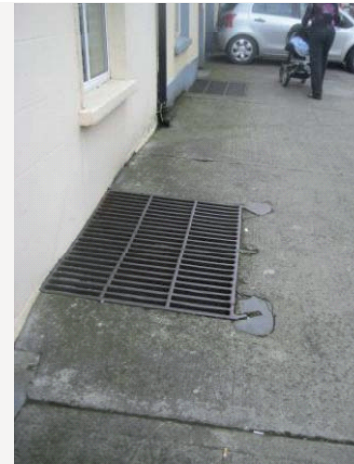
Figs 6.53 to 6.56: cast iron coal holes within pavements



Figs 6.57 to 6.60: Historic stone walls, including boundary wall to St. Mary's Col (6.59) and milestone alongside boundary wall to Cord Cemetery (6.60)



Figs 6.61 to 6.63: Limestone steps and entrance details to buildings along Fair Street.



Figs 6.64 to 6.75 (top left to bottom right): **Historic paving elements - kerbs, drains paving**
To be retained as part of public realm enhancement schemes

6.6.3 The Lanes

The Lanes in Drogheda Heritage Quarter reflect the historic urban structure, contribute to the ambience and interest of the area and increase the permeability of the centre. However, as they are generally long, shady, narrow, with little passive supervision, they can be intimidating to use. Scholes Lane is a good example of how these qualities can be mitigated to create an attractive and intriguing route, and the following design criteria could be rolled out into the other lanes:

Surfacing: Coherent textured paving surface that suits the scale of the space. Well drained, with a channel to one side. Create a threshold, signaling the entrance to the lane from the main street.

Lighting: Lighting design should reflect the ambience of the space, where wall mounted lights which add interest, can also lead you along the route. Light downwards or sideways, not upwards to avoid glare and contrast.

Facades: Where possible, encourage facades to open onto the lane, avoid blocked up windows and doors. Simple materials, textures and colours on blank facades will give coherence to the route.

Signage: Allow signage to project perpendicular to the walls so it can be seen along the route.

Views: The lanes frequently terminate with a view of a doorway or window feature of a building – emphasise these qualities – for instance in the colour of a door. Where these features do not exist, place street-scene elements such as a tree, signage, or feature light.

Nature: greenery greatly improves the ambience and biodiversity of these routes, and where it does not overhang from an adjacent site, it could be created with climbing plants and window boxes.

Management: Management of paving and lighting, cleanliness and bin storage will ensure that the lanes are pleasant routes to use.



Fig. 6.76: Bessexwell Lane illustrates some of the negative characteristics of Drogheda's lanes



Fig. 6.77 The surfacing and feature lighting greatly improve the quality of this restricted section of Meat Market Lane



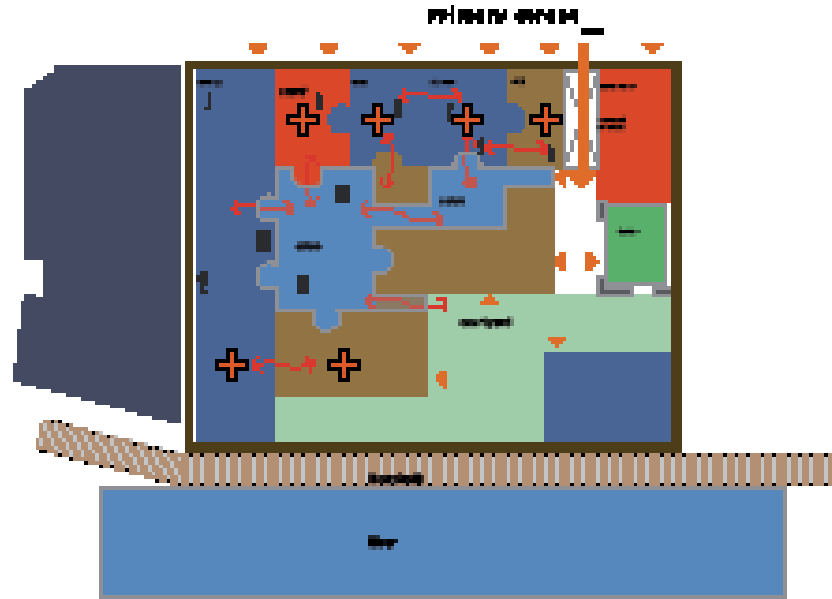
Fig. 6.78: Scholes Lane illustrates many of the qualities of surfacing, facades, signage, greenery, views and management that make it an attractive and characterful route.

6.7 'Scenario' Schematic Proposals

This section sets out a number of schematic diagrams, or sketch scenarios, for specific sites within the Heritage Quarter.

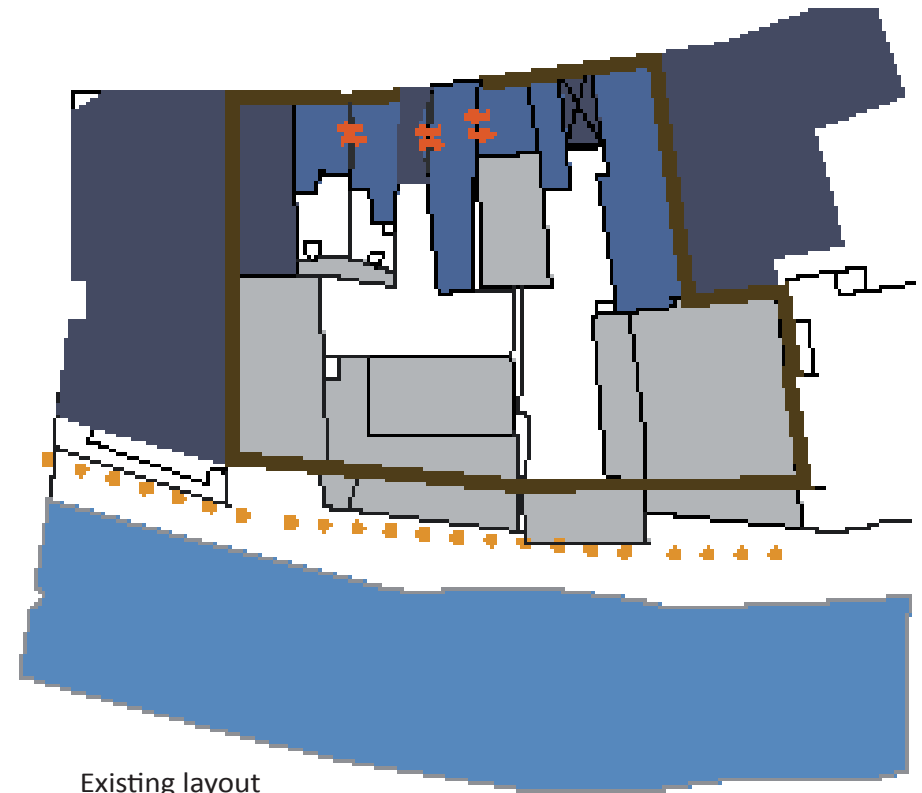
The sites are chosen to illustrate how the guidance in the Framework Plan might apply for specific situations. The proposals are intentionally schematic in nature, as there may be alternative appropriate

approaches which also meet the guidance and overall objectives of the Framework Strategy.



Scenario Diagram 6.3

- Existing Building - protected structure
- Existing Building - to be retained (not protected)
- New Building
- New public/semi-public
- Link structure/atrium

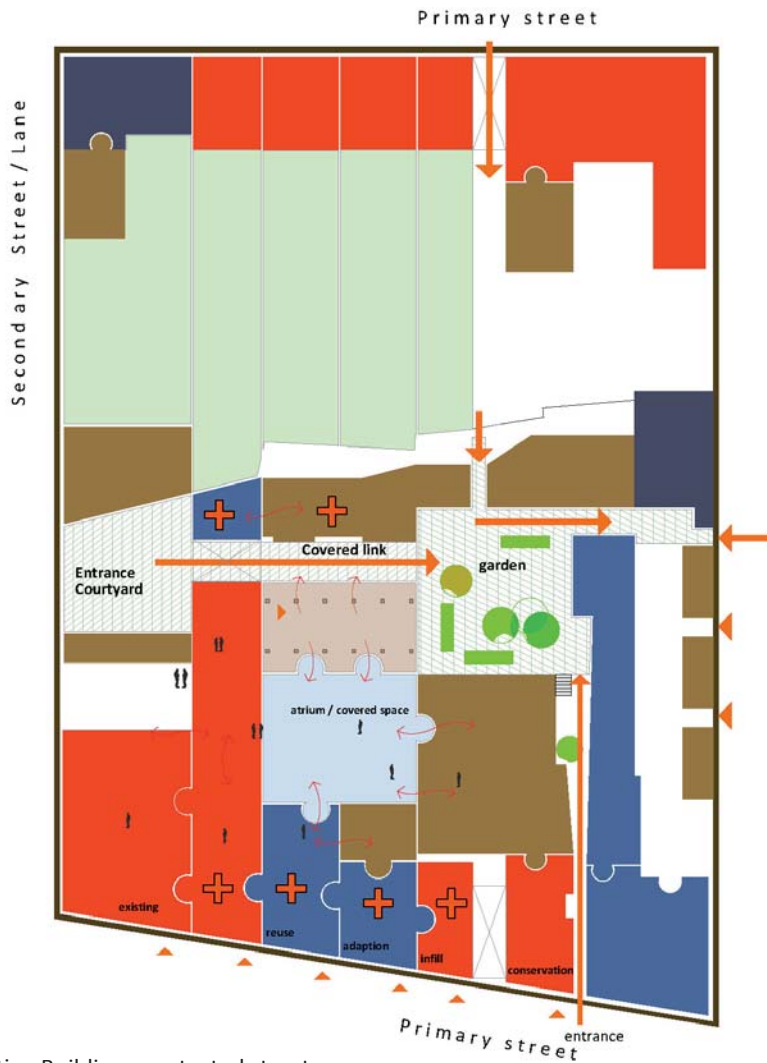


Existing layout

6.7.1 Dyer Street / Riverside:

Summary of proposal:

- Existing buildings are retained, repaired and adapted for new use
- Potential to connect existing buildings (assess suitability of protected structures for interconnection)
- Opportunity exists for specimen tree planting
- Connect existing buildings to new buildings via link spaces which may be enclosed atria or partially open courts
- Facilitate extension of boardwalk along river edge as a public pedestrian walkway
- Facilitate pedestrian connection from Dyer Street to new boardwalk via existing archway & new open spaces
- Open spaces arranged to facilitate south-facing aspect onto river and allow for active uses in new and existing buildings fronting onto this new space. Ground floors can ideally accommodate public uses with upper floors suitable for residential, office, or mix of similar uses
- Line of medieval walls to be demarcated within public realm design.

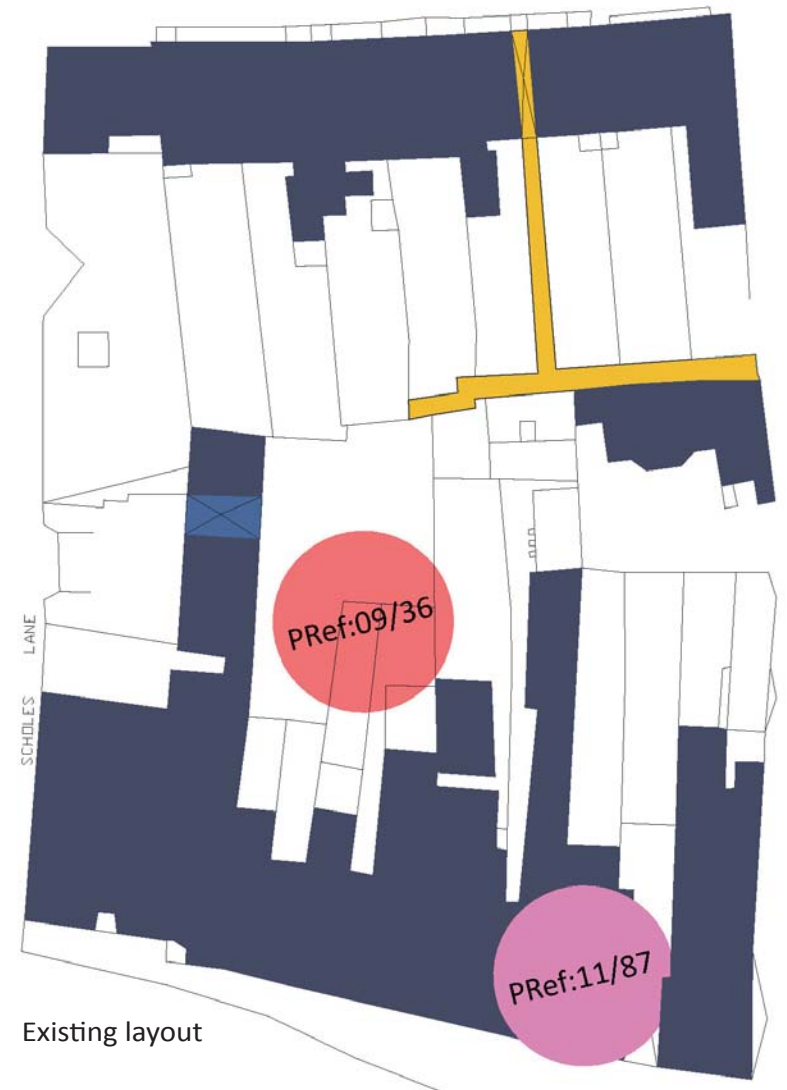


Scenario Diagram 6.4

- Existing Building - protected structure
- Existing Building - to be retained (not protected)
- New Building
- Existing gardens
- Link structure/atrium
- New courtyard/public space with native planting

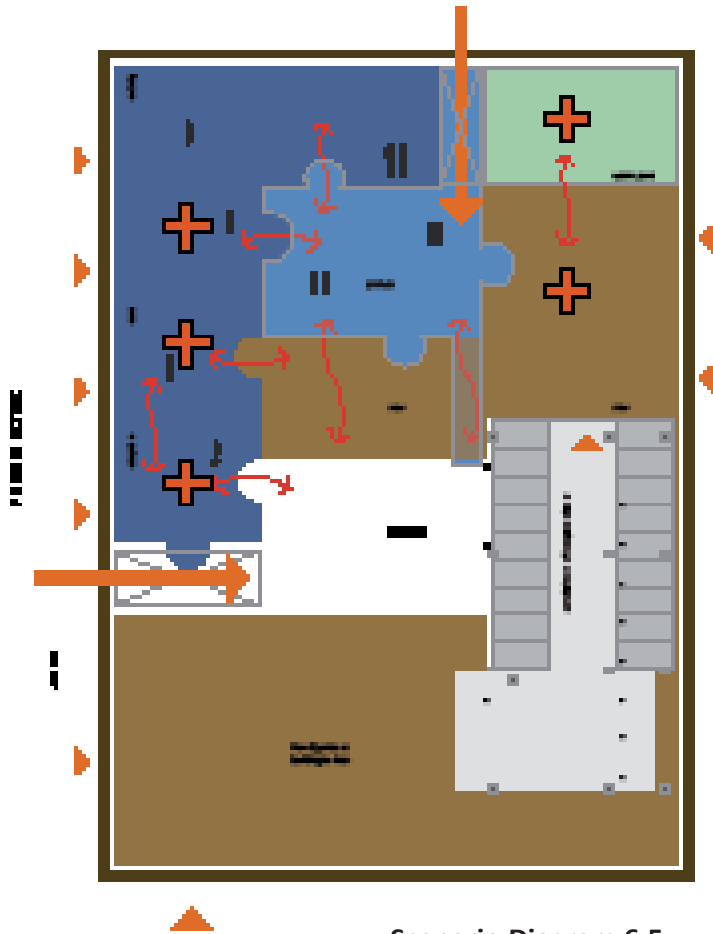
6.7.2 Narrow West Street / Scholes Lane / Fair Street

- New development stitched into backlands using existing archways and lanes for access
- Permeability provided through pedestrian link lanes and public/semi-public landscaped courtyards
- Link structures connect existing and new buildings
- Historic garden plots to Fair Street properties



Existing layout

- Scale of new development to follow scale strategy/guidance
- Potential to link/amalgamate existing buildings along Narrow West Street
- Potential for living over the shop along Narrow West Street and residential/office/institutional in new backland development.

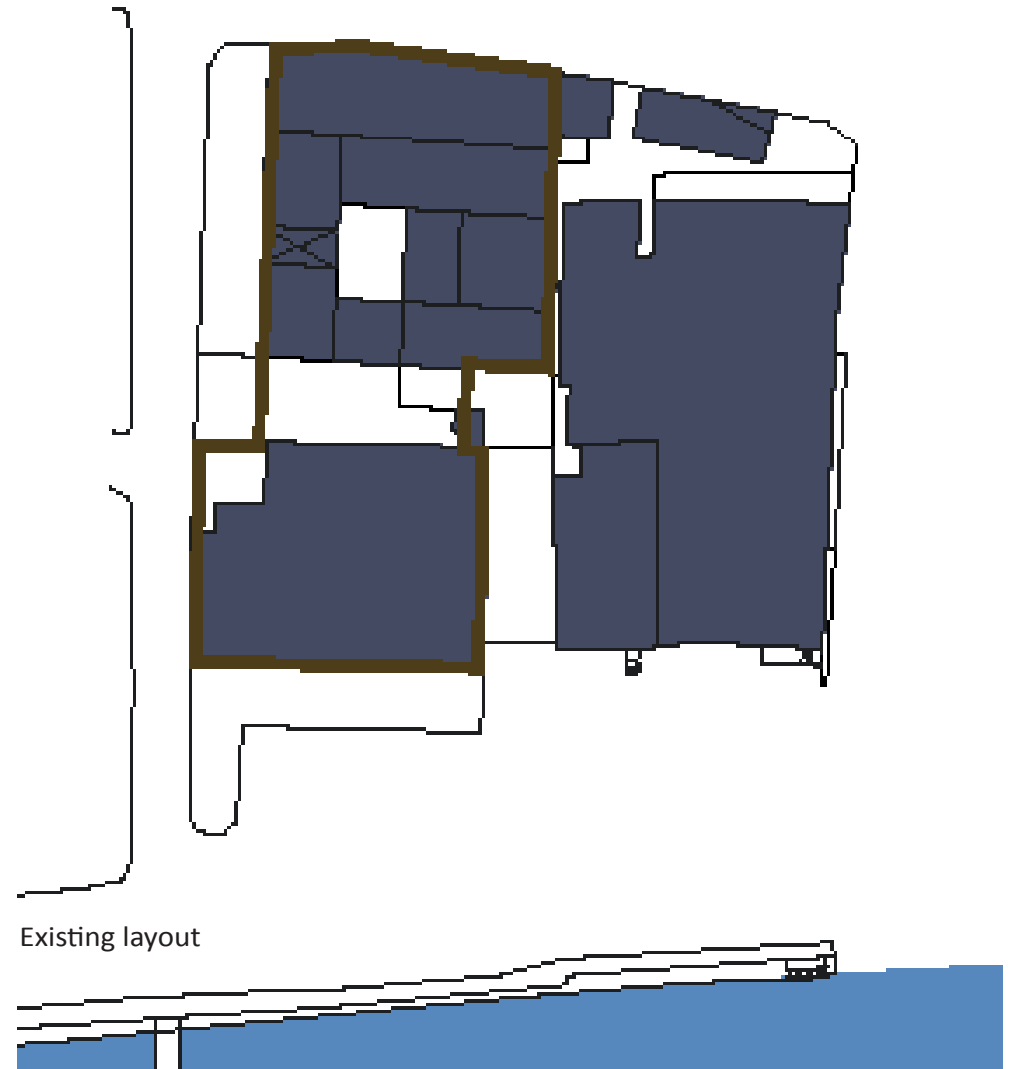


Scenario Diagram 6.5

- Existing Building - protected structure
- Existing Building - to be retained (not protected)
- New Building
- Existing gardens
- Link structure/atrium
- Off-street parking/service area

6.7.3 The Mall /Mayoralty Street / Bachelor’s Lane

- Opportunity to substantially redevelop buildings fronting onto The Mall to improve urban significance of this space and exploit south facing orientation overlooking river
- Refer to public realm scenario proposals for The Mall



Existing layout

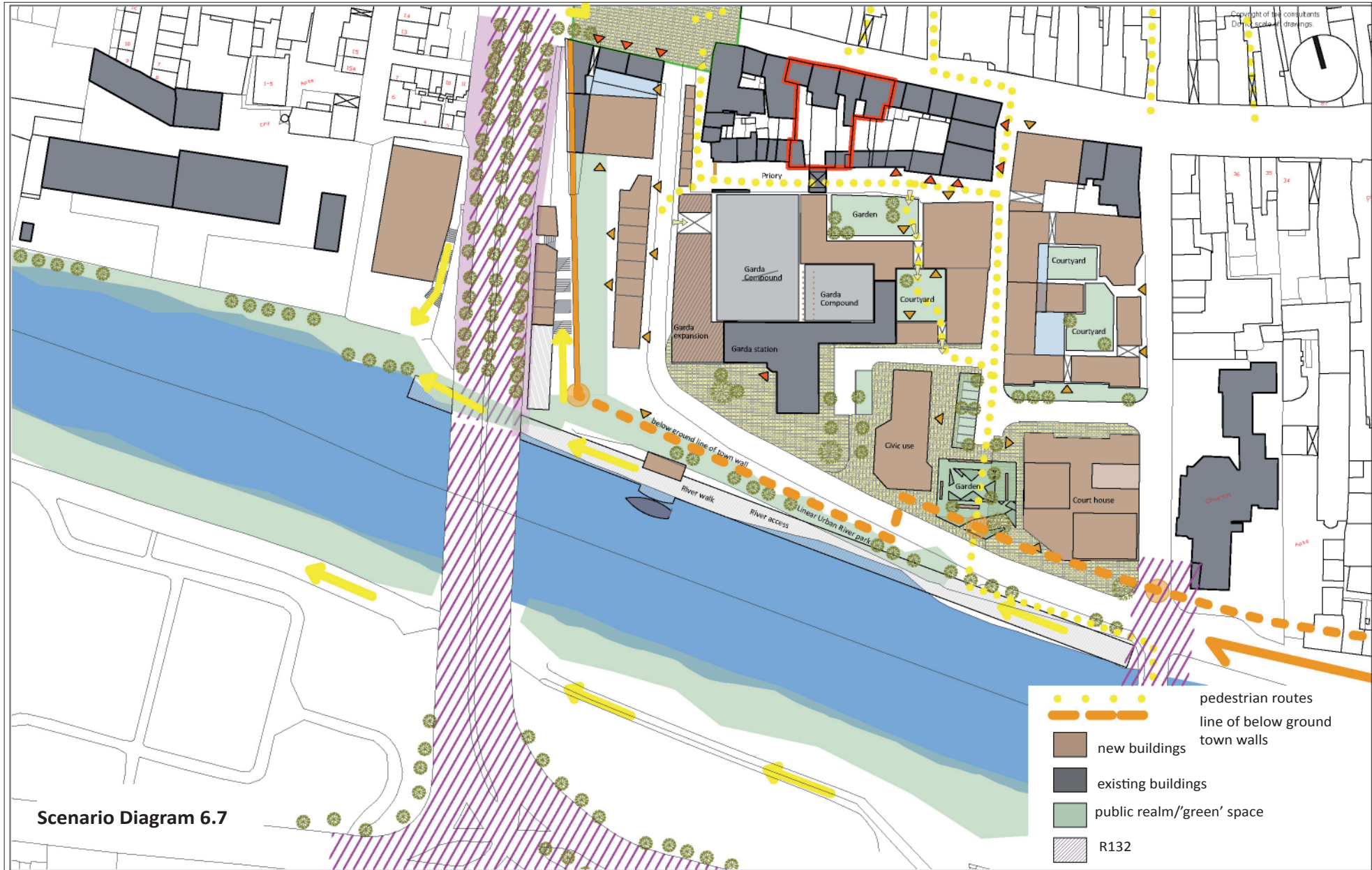
- Existing warehouse structure retained and adapted for new community/cultural uses and linked, via atrium/court to new buildings
- Green court inside boundary wall at corner - wall maintains street edge while enclosing a protected private space behind providing amenity to users within development
- Some off-street parking provided.



Scenario Diagram 6.6

6.7.3 The Mall /Mayoralty Street / Bachelor's Lane - wider context

- Improvement of public realm environment to lanes (Ref section 6.6.3)
- Potential to develop new buildings fronting onto lanes
- Strategically located active frontages onto lanes can provide sufficient animation and passive surveillance to improve character and make lanes more attractive for residents, visitors alike.

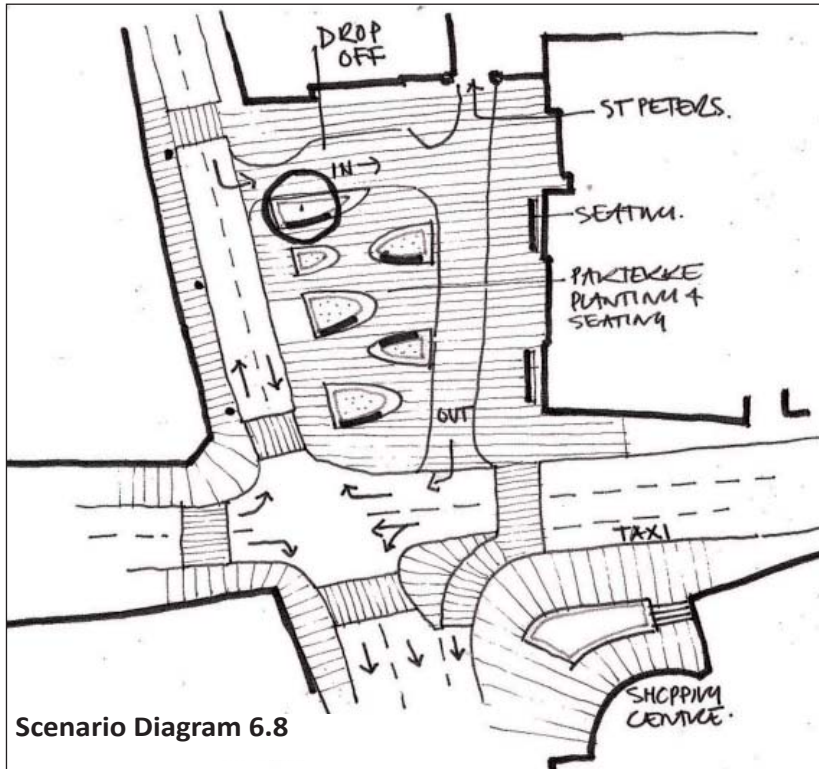


6.7.4: Abbey Lands scenario: This scenario addresses a large area, including a number of the opportunity sites identified in the Town Plan.

Following are key proposals:

- Repair and improvement of Abbey and Abbey Lane
- New buildings, including courthosue and other potential civic buildings, to create public spaces, street edges and improved river edge/linear river park
- New structure and public realm improvements to facilitate access and viewing of Town Wall and ameliorate impact of R132/Peace Bridge.
- All riverside development to be high architectural quality.

6.7.5: St Peter's Hill



Scenario Diagram 6.8



Fig. 6.79: St. Peter's Hill - existing condition

St. Peter's Hill - schematic plan showing:

- potential to introduce semi-formal planting to provide structure and shelter from adjacent traffic
- Access and drop off points provided
- Paving to extend across roadway with paved crossing points, acknowledging space extends to that enclosed by street buildings.
- Seating in sheltered areas, exploiting southerly aspect and dramatic views



Fig. 6.80: St. Peter's Hill - scenario proposal image

6.7.6: Bull Ring / R132 crossing



Fig. 6.81: Bull Ring/R132: Existing condition

Schematic Proposal below indicates:

- Improved arrangements for pedestrian crossing of R152, which currently severs Millmount, River and town centre, including rearranging left turn from R152 towards St. Mary's Bridge to increase pedestrian area
- Tree planted median - reduces impact of roadway and provides safer crossing path.



Fig. 6.82 1911 OS Map



Fig. 6.83: late 19th century photograph
(www.nli.ie)



Fig. 6.84 Bull Ring/R132 scenario proposal image

6.7.7: The Mall



Fig. 6.85: The Mall - existing condition

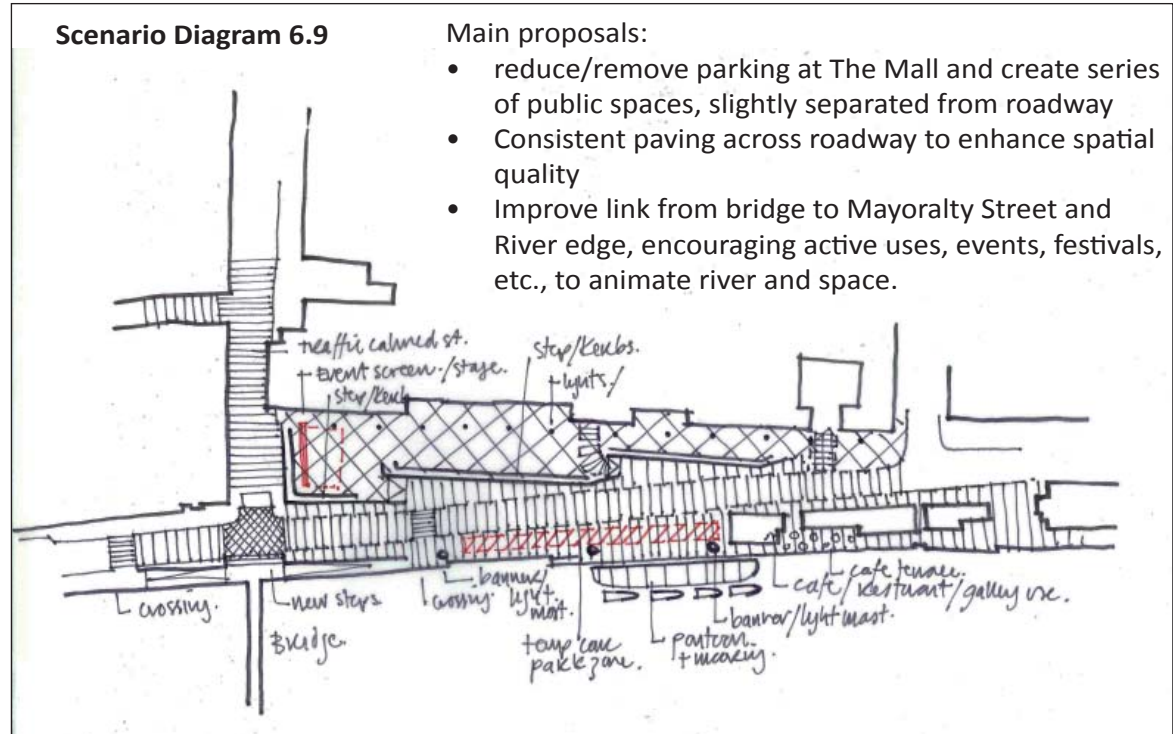


Fig. 6.86: The Mall - 1911 OS Map



Figs 6.87-6.91: Clockwise from top left: Clarion Quay, Cork; Campshires Dublin; Hamburg, Germany; Castlebar, Co. Mayo; Vienna, Austria



6.7.8: Bolton Gate



Fig. 6.92: Open space at Bolton Gate - existing condition

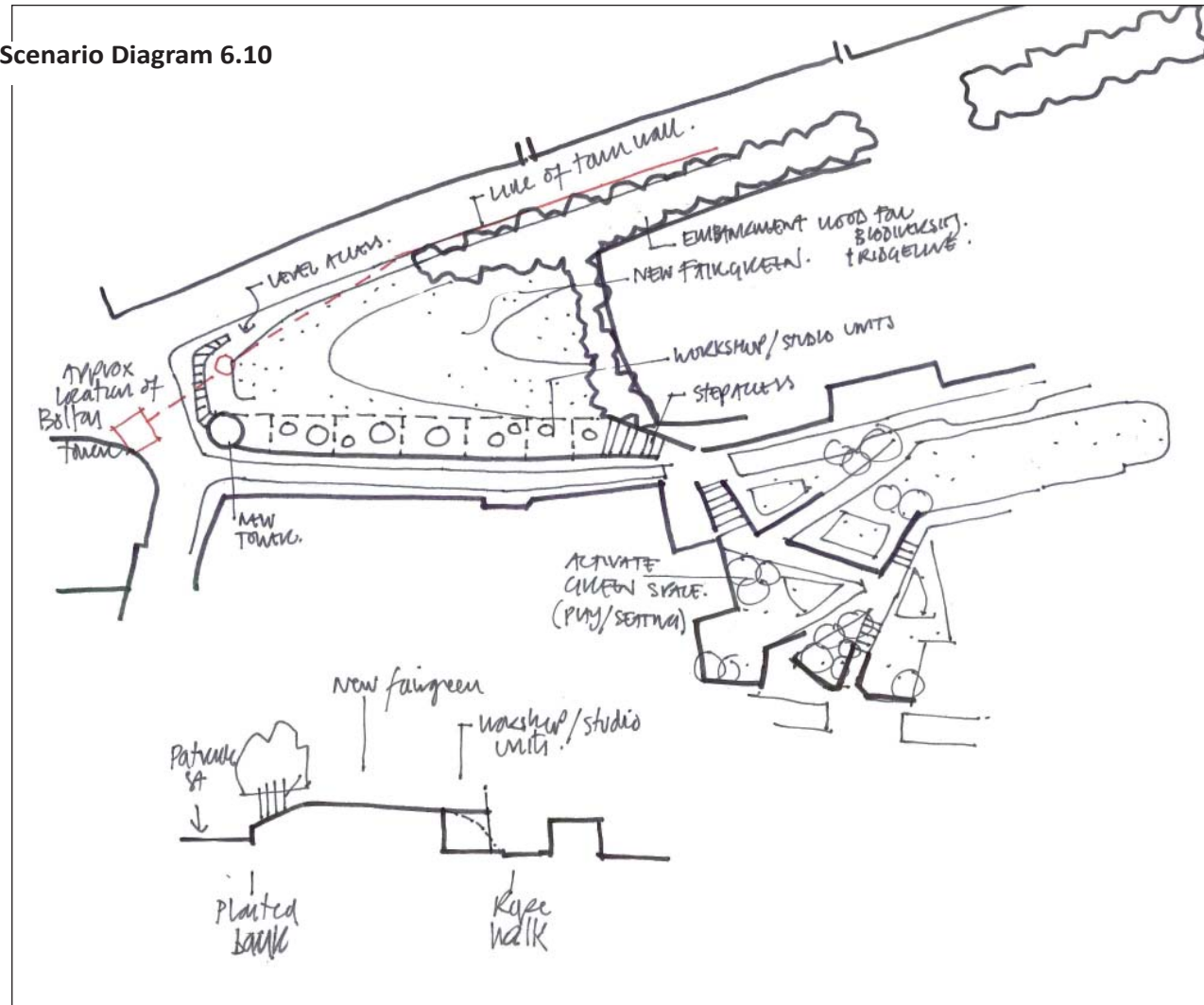


Fig. 6.93: Bolton Gate - 1911 OS Map



Fig. 6.94: Green Roof/public space - Giant's Causeway Centre

Scenario Diagram 6.10



Main proposals:

- Reform space to exploit topography and level differences
- Opportunity to construct low height building within contours as part of the public space - roof and walls encompassing this space. Building function should be compatible with space - e.g. recreational or educational use.
- Represent line of Town Walls and Gate in layout and design
- Space allows opportunity for innovative play spaces combined with active uses and passive surveillance.

7.0 Towards Implementation

Following are headings for implementation (to be completed). Much of the following reflects proposals arising from consultation processes, including Steering Group Meetings:

- Establish Implementation Group to promote, guide and initiate projects arising from the Urban Design Framework Plan. This Group may co-opt external members to assist in implementation and will identify partners and resources. Dissemination of and Training in use of Framework Plan
- Design Review Processes - including Review Panels for key development sites (multi-disciplinary and specialist focus)
- Development Briefs / Masterplans for strategic sites, development and public realm (e.g. Riverside)
- Identify key partners for implementation and potential collaborative projects (e.g. Drogheda Port; OPW; Bus Eireann/Iarnrod Eireann as well as local groups)
- Commission 3-D digital model of Heritage Quarter to be used for all development proposals to assess impacts and enhance solutions
- Early Win Projects - identify and prioritise, e.g., St. Peter's Hill, St. John's housing.
- Implement programme of temporary events to animate key public spaces, in particular the riverside - collaborate with and support existing initiatives
- Appoint Design Champion / 'Town Architect' (note success of Westport and Balbriggan)
- Green Infrastructure Plan
- Improved Pedestrian and Cycle provision

- The implementation of the proposed Port Access Northern Cross Route is considered essential to the realisation of the vision and objectives of this Urban Design Framework Plan. This road can substantially reduce dependency on the historic quarter of Drogheda to provide a primary access route to a large, and growing, part of Drogheda Port and, in doing so, can reduce some of the adverse impacts of the heavy port and other traffic currently using the historic centre.
- Implement strategic plans for wider area based aspects such as:
 - Town Walls
 - River - including linkages (boating, cycle, walking, east and west to wider hinterlands)
 - Lanes
 - Abbeylands - in partnership with OPW and other relevant bodies.

Explore and Implement Potential Development Incentives and Initiatives - local, regional and national, for example:

- Development Contribution Review - implement recommendations with possible enhance provisions applied on an areas based and time-restricted basis
- Explore potential for CPO of strategic lands to facilitate delivery of area masterplans, e.g. Abbey Lands (ref. Waterford City)
- Explore opportunity to acquire strategic (unperforming) NAMA held lands to facilitate wider area regeneration objectives
- 'Living City' programme - seek for this to be extended to historic centres such as Drogheda

- Introduce temporary land use zoning objectives to incentivise strategic uses in under-performing areas
- Consider relaxing of development standards to achieve specific objectives, e.g as set out in section on 'living in Drogheda (6.4). Such relaxation should always and only be considered where other benefits accrue
- Instigate a number of strategic 'exemplar' projects which aim to set standards set out in this Framework Plan and lever additional similar development.
- Historic Towns Initiative - seek for this to be extended to Drogheda

Appendix A: Consultation Report

Drogheda Heritage Quarter Urban Design Framework Plan Report on Consultation



Shaffrey Associates Architects
Mitchell Associates
Stephen Little Associates
Margaret Gowen

Two consultation workshops were held on 5th December 2012 at Barlow House:

1. Stakeholder Workshop: 3 to 5 pm

Over 190 invitation letters were issued by Drogheda Borough Council to a wide range of organisations and interest groups. 16 people signed the attendance sheet. In addition to the invited stakeholders there were five officials in attendance from Louth County Council (Eimear O'Callaghan, Senior Executive Officer, Laura Ward Assistant Planner, Karen Hamilton Executive Planner, Angela Dullaghan Conservation Officer and Sinead McVerry, Senior Staff Officer) and six members of the consultant team (Grainne Shaffrey, Tomas O'Connor, Patrick Shaffrey, Mick McDonagh, Feargus McGarvey and Stephen Little).

2. Public Workshop: 7 to 9 pm

The evening workshop was open to all interested bodies and was advertised in the local paper on LMFM radio. Two people attended (Eric Boyle and James M Duffy), both of whom grew up in and remain living in Drogheda). In addition to these, Ciara King Assistant Planner and Angela Dullaghan Conservation Officer, Louth County Council were in attendance with the six consultant team members as per the Stakeholder afternoon workshop.

Format

A similar format was proposed for both workshops. However, due to the small turnout for the public workshop, the format was somewhat more informal.

The stakeholder format comprised:

1. Welcome – (with tea and coffee) – from Consultant Team lead Grainne Shaffrey
2. Presentation by Grainne Shaffrey to introduce study and emerging findings.
3. Workshop discussion around tables – each table had c.7 people with A0 OS map and A1 aerial photo of the study area along with bound A3 copy of the Drogheda Borough Council Record of Protected Structures and Architectural Conservation Areas. Four questions were put to each table, as follows:
 - a. What do you like/what works in Drogheda?
 - b. What do not like/what isn't working in Drogheda?
 - c. What ideas/solutions do you have to improve Drogheda (addressing question 2)?
 - d. What would you not like to see in Drogheda?

Approximately 40 minutes was given to the table workshop

4. Feedback session, whereby each table fed back to the entire group, their responses to each question. These were all recorded on a flip chart (with other notes taken also). The results of these are set out on following pages.

The public workshop addressed all the above items, however it proceeded in a more informal manner with discussion amongst all participants around a single table.



Feedback from Stakeholder Workshop:

Question 1 : Things you like or work well:

FLIPCHART NOTES:

- Medieval street pattern
- Social interest
- Historical interest
- Esp 'High St' of West Street
- Feels like a live/living city
- Tourism value – history and culture (Millmount and Gallery)
- St Peter's West St and 'Piazza'
- Riverwalk
- Topography - hills and flat areas and the retention of plot sizes and character because of hills (easier to modify on flat)
- Nooks and Crannies
- Floral displays – no vandalism to these, showing sense of maturity and
- Civic pride
- Ramparts/ St Dominic's Park for recreation and play
- Churches for architecture and cultural interest – Christian heritage
- North – South Route through town for its vistas (from relief road/Peace Bridge) (getting a sense of the town) Laurence's Gate
- Millmount and surrounds – strong sense of community and use by the community – social, economic and cultural – a distinct neighbourhood
- Corner buildings and grain
- Architectural Detail

Question 2 : Things you don't like or don't work well:

FLIPCHART NOTES:

- Buttergate – 'What is it? No interpretation etc
- Drogheda turns its back onto river
- Neglect of Abbey area
- St Peter's, George's Sq North Mall – under used - these key areas should be for pedestrians, not car parking
- Narrow West St/Abbey also under used
- Ongoing large retail development at expense of smaller retail
- 'The big shopping centres have nice shops but you find them in every town – you don't get a street like West Street (and Narrow West Street) anywhere else'
- Development overhanging river and vista along river
- Demise of West Street (economic)
- Loss of cohesion (feergus not sure what this meant exactly – related to joined up thinking)
- Traffic directions – poor
- Wayfinding for visitors – none, especially for those in cars
- Lots of signs into town, but none to help you find your way out (to M1)
- Lack of sensitivity of some new buildings – not in context of town
- Traffic congestion
- No facilities in centre (like TLT)
- Lack of pubs/restaurants
- Division across river ("farside and farside2)
- People not working together – but competing against each other
- Example – there is 1 river and 3 rescue teams

Question 3. Things that would improve Drogheda

FLIPCHART NOTES:

- Town architect or someone or team to champion the town
- (to balance engineers)
- Mcdonalds is an opportunity to stop and have views
- Designate as heritage town
- Tourist office opening - should be open at weekends (discussion about how to do this- ask Failte Ireland or get community to volunteer or place retail opportunity in tourist office)
- Be able to buy tickets for newgrange
- Emphasise it as a location with other attractions
- Drogheda should be centre of Boyne Valley experience – Oldbridge, Newgrange, canal, Monasterboice, Beulieu house – should all be linked
- City status with central administration
- Parking tickets should cover everywhere in town (so you could go from one space to another)
- Difficult to open new shops due to regulation – barriers to doing this are rates, rents, permission for change of use and time it takes and cost – could there be a waiver?
- Allow Pop Up / experimental retail
- Park outside town and walk into town. Link walk along park to new proposed bridge to west.
- Play opportunities for visitors in centre of town (example - Paris street with cafe and play equipment s you can have coffee while kids play)
- More river access
- New uses for old quarters – (music /arts and all that goes with it)
- Marys Bridge to James' St, Shop St to Millmount (improve routes?)

Question 4 – What would you not want to happen ?

FLIPCHART NOTES:

this question was omitted to allow general discussion which focussed on some key objectives:

- Work to strengths of town
- Rethink retail – include specials shops
- Implement small projects suggested by Tidy Towns committee
- Visual improvements of town – these can be simple
- Better marketing
- Traffic flows in town across river are difficult

Other comments arising from Stakeholder Discussions:

- Private buses – need location to pick up people for coaches (1,500 people per day)- could link with car park and coffee shop as form of regeneration.
- Bus parking is currently on wrong side of road for bus door
- Congestion in town centre is a problem with particular conflict at West Street – perhaps experiment with pedestrianisation, or pedestrian hubs
- Social hub –TLT out of town, which is a problem/loss for town
- Protection of town centre has resulted in big retailers want to go out of town; exacerbated by parking problems and cost of being in town centre v cost of out of town (parking free out of town)
- Lack of knowledge of visitor where to park – lack of legibility
- Traffic flows problematic
- Social vitality at night lacking in town centre, e.g not many restaurants. West St used to be vibrant at night, not any more
- Interesting from cultural perspective – St. Peter’s, Highlanes; Barbican; Library (municipal centre)
- TLT is well marketed – need to take a leaf from this and improve marketing of centre
- Nobody giving any information on historic buildings
- Shop Street – fine grain of individual plots is very distinctive (part of what we like)
- Change of scale within ‘walls’- what we like
- Viaduct and vistas – what we like
- ‘Farside and farside’ - Competition between both sides of river is negative : road is a barrier, which articulates/underlines this.
- Recover space at St Peter’s (improvement proposal)
- Town architect – Westport, Balbriggan – champion.

Transcribed from one stakeholder’s ‘post-its’:

- Prioritise what is to be protected, e.g. ‘keep facades but allow floor plate change to reflect modern requirements
- Consider incentives – development contribution removal; rates reduction for initial period
- Allow developer make profit if private sector is to push developments
- Decide – do we want a museum or a living/working space (not mutually exclusive)
- Put the citizen at the centre of proposal – people are more important than buildings

Feedback from Public Workshop:

- DROGHEDA NOT MAKING MOST OF ITS HERITAGE – NEEDS TO WORK TO ITS STRENGTHS
- Are people afraid of adapting heritage buildings? – shouldn’t be, these are positive assets. ‘Hands are tied’ but nothing wrong with that, good thing, don’t want to change windows.
- Drogheda History is positive
- A nice town – people are friendly – you know them
- Growing up in Drogheda - played football, boxing, scouts, regattas in water – lots to do; used to play tennis on William St
- St Dominic’s Park – good park and walk to Oldbridge, but not like Tullamore Park, a real town park.
- West Street – used to be called the Golden Quarter Mile, (not anymore)
- Pub’s good in Drogheda
- Scholar’s hotel is good and doing well; not sure about D hotel; Kearns’ Bar?
- Traffic is a problem
- Walking and cycling – nice town, historic, but not so easy to cycle in – need to cycle well out in road to keep safe (both consultees cycle in Drogheda, one uses electric bike)
- Doner’s Green – a good space when growing up: good for walking
- Rope Walk – named after stretching rope along this space – used to be flatter, not sure when built up, maybe reused rubble. Used to play ‘cowboys and indians’ here
- Would be good to have a loop walk along river – especially at west end with footbridges [could develop a number of looped walks, people like circuits –visitors and walkers]
- Transport museum in council depot
- St. John’s houses adjacent to St. Peters - should be refurbished as houses
- Like living in town
- [Indecon report?]
- Lanes – some good, some not useable (Barrack Lane from Millmount).
- Free School Lane is supposed to be accessible – right of way – but gates always closed, should be enforced to stay open.
- Amenities not accessible – can’t take photo of St. Lawrence’s Gate – can only go up it once a year
- Lots of places closing – restaurant in Abbey centre closed.

Urban Design Framework Plan for Drogheda Heritage Quarter

ATTENDANCE LIST

NAME	ADDRESS/ORGANISATION	E-MAIL/CONTACT
DECLAN WALSH	DECLAN WALSH & CO CHARTERED BUILDING SURVEYOR & CONSERVATION SURVEYOR	086-8122172 declan.walsh@eircom.net
FRANKIE WATTERS	DUNDALK INSTITUTE OF TECHNOLOGY, DUBLIN RD. DUNDALK	Frank.watters@dit.ie
Ray White	Bullring Drogheda	
Billy Quinn	Drogheda Museum Millmount	bed.gunn@gnal.com
Rev Michael Graham	St. Peter's Church of Ireland	drogheda@smaghonglin.org
Andrew Murphy	Brady Hughes Consulting, Drogheda	post@bradyhughes.com
Helen Collins	Brady Hughes Consulting	helen@bradyhughes.com
Daria Collins	O'Brien Collins Estate Agent	dc@obriencollins.ie
Malcolm McEnt	McEnt Architects	info@mcintosharchitects.ie
David Millar	Icon Planning + Design	dmillar@eircom.net
Paddy Matthews	Matthews Coaches	paddy@mattkews.ie
EMMET HUMPHREYS	ARCHITETS, WEST ST.	eh@ehadr.ie
Aignes Joyce	Business Fine Feathers ^{Nearway} West St	nearway Rd Monasterevin
Stephen Oppermann	OPPERMANN ASSOCIATES	stephen.oppermann@oppermann.ie
EDDIE PHILLIPS	VCL CONSULTANTS	eddie@vcl.ie
Aimee McKenna	Drogheda City Status	aimemckenna12@yahoo.com

Stakeholder Workshop
Attendee List

Urban Design Framework Plan for Drogheda Heritage Quarter

ATTENDANCE LIST

NAME	ADDRESS/ORGANISATION	E-MAIL/CONTACT
ERIC BOYLS	HARPER HOUSE, WILLIAM ST. DROGHEDA	boyleeric@eircom.net
JAMES M DUFFY	17 TRINITY GARDENS DROGHEDA	086-82-42-702

Public Workshop
Attendee List

Duffy's 20/20 vision for the future.

It is only 15 years away !

Suggestions to make Drogheda "The Jewel of the North East", a Town to be proud of:

- 1. Abandon plans for Fire Station at Rathmullen Road, due to difficulty at exit of Marley's Lane to Donore Road Junction.**

Relocate to Buttergate site, opposite Bus Depot, Donore Road. Access to all areas from this venue.

Retain and upgrade Fire Station at Scarlet Street for north side.

- 2. Refurbish and clean up Buttergate. It is a part of our Heritage.**
- 3. Driving Test Centre.**
- 4. Crematorium.**
- 5. Refurbish Lourdes Stadium & Tennis Club.**
- 6. Railway Station to be opened on north side of town.**
- 7. Marina and Rowing Club.**
- 8. Re-opening of Canal, Oldbridge to Navan.
Barge trips at weekends/bank holidays.**
- 9. Park & Ride facility, incorporating "Tourist Coach Park" outside town, both north & south side.**
- 10. Show Jumping Arena.**
- 11. Cycling Velodrome.**

12. Sports Centre incorporating the following:

- (a) Go -Kart Track.**
- (b) Ice Rink.**
- (c) Ten pin Bowling.**
- (d) Skate Board facility.**
- (e) Roller Blade & Roller Skate facility.**
- (f) Public Handball alley, indoor & outdoor.**
- (g) Bowling Greens, indoor & outdoor.**
- (h) Gymnastics Club.**
- (i) Table Tennis & Badminton.**
- (j) Cricket Green.**
- (k) Squash Club.**

13. Activity Centre, incorporating the following:

- (a) Climbing Wall.**
- (b) Sky Cables.**
- (c) Cycle Hire & Canoe Hire**
- (d) Obstacle Course.**
- (e) Archery.**
- (f) Paintball.**

14. Removal of Bolton Street Market & Corporation Maintenance Yard(Greenlanes). Possible relocation site – Old Centronics Factory, Donore Road Industrial Estate. Plenty of on site parking.

**No loss of revenue at Bolton Street Car Park on Saturdays.
No obstruction to Fire & Ambulance Service for residents of Bolton Street/Greenlanes area.**

- 15. Old Corporation Maintenance Yard – Convert to Fire Station Museum, incorporating Transport Museum/Model Railway Exhibit.**
- 16. Opening of Northern Cross Route as soon as possible.**
- 17. Swing Bridge from Tom Roe’s Point to New Colpe Road.**
- 18. Ferry Service from Tom Roe’s Point, serving Isle of Man, Fleetwood and Liverpool.
Drogheda is the shortest route to the UK.**
- 19. Revival by local traders of the “Bonanza Week”.**
- 20. Replace Band Stand in St Dominick’s Park.**
- 21. Concert Hall Facilities – similar to The Point Depot.**
- 22. Alpine Retirement Village, to provide a tranquil setting for Residents. This could provide on site medical and recreational facilities. Would also accommodate resident’s pets.**

To: Grainne Shaffrey
Cc:
Subject: FW: drogheda heritage quarter
Attachments:

-----Original Message-----

From: Eric Boyle [<mailto:boyleric@eircom.net>]
Sent: 10 December 2012 15:17
To: Studio
Cc: angela.dullaghan@louthcoco.ie
Subject: drogheda heritage quarter

Hi Grainne,

I just want to express how much I enjoyed the workshop in Barlow House regarding the introduction of the Drogheda Heritage Quarter. This is an excellent idea to help to promote the importance of the historical heritage of Drogheda, which is something I have a high interest in.

It will be necessary to make the historical sites in Drogheda such as St. Laurence Gate, Magdalen Tower, The Old Abbey, Butter Gate and other various sites more easily accessible. The traffic management issues should not interfere with this work as the traffic problems in Drogheda have always been there and no matter how much the Botough Council do, the traffic problems stay.

An idea I had was a stone line, marking the route of the old town walls, in the ground which people could follow the outline of the old town.

I have many other ideas and I know of other people in Drogheda that share the same interest in Drogheda heritage that I do.

I will keep in touch through this process.

regards

Eric Boyle
Harpur House

--

The Urban Design Framework Plan for Drogheda Heritage Quarter Submission on behalf of Matthews Coach Hire Limited

Introduction

Two representatives of Matthews Coach Hire Limited, including the Managing Director Mr. Paddy Matthews, attended the Public Workshop held on 5th December 2012 and participated in the round table consultation process.

Matthews Coach Hire, was founded in 1995, by the Managing Directors Paddy & Mary Matthews. Together with the help of their three sons and their employees, Matthews Coach Hire, provide an excellent and efficient service to their customers both in their private hire and daily commuter services.

The company identified the potential of the M1 when opened and commenced the commuter service between Dundalk – Drogheda – Dublin in May 2004 and added more times as demand for the service increased. Always having their passengers in mind they constantly strived to add more to their passengers experience an example of this is introducing free WiFi on all service. In 2007 they added the Bettystown – Laytown – Dublin service and currently the company carries in total circa 4500 passengers daily on the commuter services. About 2,000 of these passengers travel to and from Drogheda.

Matthews Coach Hire Ltd is based in Ireland and is a member of The Coach Tourism Council of Ireland (CTTC) providing Coach Tours of Ireland and the UK and Commuter Services. All our vehicles are Failte Ireland approved.

The company has also developed a significant coach tour business and recognises the potential of the cultural/historical sector as a market for the development of such services. This has particular significance in the context of the Boyne Valley area, including New Grange, the Battle of the Boyne site, Slane, Monasterboice and obviously the historic town of Drogheda itself.

Purpose of the Project

Some general comments

The company notes that purpose of the project is to set out the principal objectives to guide the sensitive future development of the Heritage Quarter of Drogheda To achieve this objective, particularly in the context of the current economic situation, it will be necessary to protect the heritage assets of the town whilst developing the economic, recreational and tourism potential of Drogheda having regard to its natural setting and heritage and promote the continued development of the Heritage Quarter as a place to invest, work, live, recreate and visit.

Having attended the said Public Workshop and listened to the various and diverse contributions it is clear that to achieve the objectives of any Framework Plan for Drogheda Heritage Quarter that emerges will require an imaginative and perhaps radical approach. This is so not least because of the probable absence of any significant funding to underpin any such plan.

It appears realistic to suggest that the regeneration of the inner town area, centred on West Street, to its former status as the retail centre of Drogheda will not occur. Therefore alternatives need to be examined. There is already a significant level of vacant commercial premises within the Drogheda Heritage Quarter and indeed when the Narrow West Street, Georges Street, Trinity Street and Fair Street area is examined the issue becomes one of ‘dereliction’ in some instances rather than simple a vacant building. (‘A Gateway to the Drogheda Heritage Quarter’?)

In the absence of either public funding or significant private funding what can be done to protect the surviving physical heritage within the ‘Drogheda Heritage Quarter’ as the danger of a vacant property is that it becomes a derelict property and intimately this will lead to further loss of the remaining physical features that form part of the unique historical heritage of Drogheda?

Some initial suggestions include:

1. Develop a series of historical/ heritage walks within the ‘Drogheda Heritage Quarter’ centred on a number of identified landmarks such as Millmount, St Peter’s Church, the Laurence Gate etc.
2. Such ‘walks’ to be accessed through a number of ‘gateways’ such as St. Georges Square/Narrow West Street and to be ‘signposted’ by means of inserts on footpaths, plaques on buildings etc.
3. Develop a designated Tourist Information Centre to be open year round but in particular when tourists need access to such a centre, i.e. over holiday periods and Public Holidays. Any such facility to be located at one of the identified ‘gateways’ to the ‘Drogheda Heritage Quarter’.
4. Be realistic as to the future commercial activity that will take place within the ‘Drogheda Heritage Quarter’. As stated above it will not prove viable to restore the West Street area as a significant retail area in Drogheda but this does not mean that further decline should be facilitated either.

5. The type of ‘commercial activity’ that could be facilitated would include:
 - (a) ‘Pop up shops’
 - (b) Street markets for example outside St. Peters Catholic Church and at Georges Street/Narrow West Street.
 - (c) Target activities that will generate activity and ‘footfall’ outside of the standard retail trading times such as restaurants, artistic and cultural activities, guided heritage walks etc.
6. To facilitate the above will require the co-operation of:
 - (a) Owners of properties in the ‘Drogheda Heritage Quarter’ who will need to be realistic about issues such as rental values and the availability of short terms leases.
 - (b) Drogheda Borough Council (shortly to become simple Louth County Council) who should at least examine issues such as planning matters issues and rates payment waivers (for designated period of initial set –up of new businesses) in the Drogheda Heritage Quarter’. A simple example would be a relaxation of restrictions on the use of street furniture by food outlets and licensed premises.
7. The continued viability of the Heritage Quarter as ‘a place to invest, work, live, recreate and visit’ cannot be considered in isolation from its broader geographical and historical setting within the Boyne Valley. Visitors to the Boyne Valley at the moment appear to be focussed, either by personal choice or by direction, to specific heritage venues within this area. Mellifont, New Grange, Slane, Monasterboice, the Battle of the Boyne site and Drogheda Heritage Quarter must become elements in a broader picture. This can be facilitated by such measures as :
 - (a) To date the emphasis has been on marketing the individual features of the Boyne Valley rather than identifying the Boyne Valley as a total package or experience in itself. This emphasis must change and this requires the co-operation of both national and local bodies but the lead must come from the currently divergent and separately focussed local bodies who are trying to promote their own particular ‘product’.
 - (b) Why can’t a tourist/visitor to the Boyne valley buy a ticket that will provide access to all attractions within the Boyne Valley including appropriate transport links?
 - (c) There is a need to develop a specific transport method that will service all heritage sites within the Boyne Valley, the Boyne Valley Hop-on/Hop-off bus? It should be remembered that access to all the identified sites is primarily by means of car transport

and there is not even a guided and signposted route between the sites.

- (d) There should be an annual ‘Boyne Valley Heritage Week’ that would examine and detail the current and developing historical and archaeological discovery of the Boyne Valley. Recently representatives of this company attended separate events regarding the Battle of the Boyne site, Slane and shortly a seminar on the Newgrange/Knowth/Dowth complex. Why were all of these events not held in conjunction with each other?

Observations from the perspective of Matthews Coach Hire Limited as a company that operates a commuter service to and from Drogheda

1. As stated above this company moved significant numbers of commuters into and out of Drogheda on a year round basis. It would be a reality that these commuters are comprised of individuals travelling to work, students attending third level colleges in Dublin and Dundalk, those attending medical facilities in both Drogheda and Dublin, those attending specific entertainment and sporting events in Dublin and also shoppers primarily travelling to Dublin.
2. However this company also recognises the potential of the cultural/heritage sector in the context of our coach tour operations. We have watched with some despair the continued under-utilisation of the potential of the Boyne Valley in this respect. The reasons for this include and some of the solutions are:
 - (a) A lack of co-ordination as between the various interested parties who are attempting to promote their own individual product to the detriment of the overall package that is the Boyne Valley.
 - (b) The absence of a coherent transport infrastructure that would facilitate the co-ordination of the above diverse heritage sites and locations.
 - (c) The lack of a ‘focal’ point for the Boyne Valley and it is possible that the Drogheda Heritage Quarter could fulfil this role with the provision of a central tourist information centre for the Boyne Valley, be the origin and destination of an integrated public transport facility for the Boyne Valley (Boyne Valley Hop-on/Hop-off bus), provide a wide range of accommodation for visitors and be itself through the Drogheda heritage Quarter be part of the cultural and entertainment aspect of any visit to the Boyne Valley.

- (d) The possibility of developing some of the derelict buildings in the narrow West Street/Georges Street/Fair Street area as 'hostel type' accommodation should be examined. The attraction of Ireland as a destination for 'back packers' should not be under-estimated and these visitors require 'hostel' type accommodation. The Boyle Valley is exactly the type of destination such visitors will identify with but currently there is little or no suitable accommodation for them in this area. Again the Drogheda Heritage Quarter could become the focal point for such visitors with a combination of heritage, accommodation, restaurants, pubs and other entertainment centres.
- (e) The Drogheda Heritage Quarter should be developed as the 'gateway' to the Boyle Valley in the context of its unique position on the M1 North/South Corridor.

Specific observations in respect to transport access to the Drogheda Heritage Quarter:

- 1. As early as 2006 this company actively engaged with the West Street Environmental Improvement Scheme. It would be correct to state that we predicted that the effective removal of commuter bus transport services from West Street would be detrimental in the long run. This included the services offered by this company.
- 2. We noted the apparent lack of any reference in the course of the public consultation session to the development of public transport facilities as part of the regeneration of the Drogheda heritage Quarter. The development of such facilities is, in our view, central to the success of any proposed plan for this area as detailed in this submission.
- 3. Also in 2006 we made a submission in respect to the Drogheda Transportation Study which included a number of meetings with the consultants involved, WPS. In effect we proposed a number of radial local bus routes for Drogheda to be centred to a large extent on West Street or the 'gateways' to West Street at Georges Street and Peter Street.
- 4. Whilst the statistical information in the attached document dates from September 2006 there are no intervening factors that would indicate an increase in the movements identified. Indeed the reality is probably that the decline of West Street as a retail destination, in the context of the development and opening of the new retail centres in Drogheda, would indicate that the figures are now an over-estimation of the movements concerned.

- 5. These proposals were never realised but one of our recommendations, i.e. a dedicated bus stop at Georges Street where the current parking area is located has 'survived'.
- 6. Such a stop would facilitate services such as ours which operate along the M1 Corridor via Drogheda but would also facilitate all other services, local and regional which operate past the Georges Street/Narrow West Street 'gateway' to the Drogheda heritage Quarter.
- 7. In the interest of clarity we attach, as Appendix 1 to this submission, a copy of our proposal to Drogheda Borough Council in respect to this matter, dated 1st October 2006.
- 8. It is our understanding that the National Transport Authority (NTA) has approved a sum of €30,000 for Drogheda Borough Council to carry out the required works at Georges Street.
- 9. However Matthews Coach Hire see this as only a part of an overall plan to utilise the Georges Street 'Square' as part of such a gateway to the Drogheda Heritage Quarter. Issues such as:

- (a) Those items already identified above should be carried out as a consequence of the proposed new bus stop.
- (b) Could the existing public toilets located at Georges Street be converted to use as a dedicated tourist office or alternatively why not locate such a facility in the building where the public consultation meeting was held?
- (c) The fact that the location of the bus stop will effectively make this area a 'gateway' to the Drogheda Heritage Quarter for visitors clearly points to this been a commencement point for the dedicated historical and cultural walks suggested above.
- (d) In this context Matthews Coach Hire commissioned a projected image/plan of the area with the bus stop included but also including a street market etc. This will be available shortly and will be forwarded as soon as it becomes available.

Appendix 1

The names of identified individuals have been removed from this correspondence

1st October 2006
Drogheda Borough Council,
Fair St.
Drogheda,
County Louth.

Dear -----,

SUBJECT: PROPOSAL FOR A BUS STOP AT GEORGES ST. TO ACCOMMODATE SOUTHBOUND BUS TRAFFIC.

As you will recall we have raised the issue as to provision of a bus stop at Georges St. to accommodate southbound bus traffic movement with you on a previous occasion. As part of ensuring the quality of the service that we provide to commuters to and from Drogheda we keep under constant review the effect of West Street Environmental Improvement Scheme. It should be pointed out that ----- and ----- have been very helpful in respect to any issue that we have raised with them regarding the said works. Although outside their remit, at the most recent meeting with both ----- and ----- we again raised the obvious advantages that would accrue to traffic movement in West St., both in the short and long term from the availability of a south bound bus stop at Georges St. They accepted the validity of our arguments but suggested that the matter should be raised directly with you.

In this context we carried out a survey of the both the traffic use of the current parking facility at Georges St. and the pedestrian use of the adjoining footpaths on Friday 8th September from 8.30 am to 5.50 pm. The day and date was chosen, as it is probable that Friday represents the busiest shopping day and also all schools would have fully re-opened at this stage. We accept the obvious limitations of such a survey but it does give a reasonable representative 'snapshot' of traffic and pedestrian activity in this location.

The outcome of the survey provided the following details:

1. Pedestrian Movement.
 - (a) The total number of pedestrian movements on the footpath to the front of the existing 9 parking spaces was 425, with 311 occurring on the footpath to the rear of the parking spaces.
 - (b) Peak movements occurred as follows:

Time	Front	Back
8.45 – 9.00	18	20
10.30 – 10.45	18	19
13.45 – 14.00	27	9
14.45 – 15.00	20	18
15.15 – 15.30	17	14
15.45 – 16.00	13	14
 - (c) It appears that these 'peaks' coincide with pedestrians going to and from both work and schools.
2. Use of the 9 car parking spaces currently provided at Georges St.
 - (a) At no time throughout the day were more than 4 car parking spaces utilised at the same time.
 - (b) In total 23 vehicles utilised the spaces throughout the day.
 - (c) The spaces remained totally empty from 11.05 – 12.00.
 - (d) The 'peak' usage was as follows:

Time	No. of spaces occupied
1.38 – 1.52	4 spaces
3.27 – 3.40	4 spaces
3.45 – 4.05	4 spaces
 - (e) Throughout the day, there were 7 traffic movements related to the businesses to the rear of the spaces, either delivery or pick-up. These traffic movements did not utilise the 9 spaces.

In the context of the above we are suggesting that it would be feasible to provide an appropriately designed bus stop at Georges St. to accommodate southbound traffic movement without such a facility significantly effecting pedestrian use of a redesigned 'front' footpath, diminishing to any identifiable extent the overall provision or use of car parking facilities in the inner town area centred on West St nor hindering the activities of the businesses located to the rear of the existing facility.

We attach a draft proposal for such a bus stop. It is our estimate that redesigned facilities such as we propose would leave a total of 3 parking spaces, one of which would be for disability access. This would also facilitate the continued commercial activity to the rear of the spaces including vehicle movement directly related to that activity. In the context of the identified pedestrian movement, the setting down and picking up of passengers at a revised footpath layout would not present either an obstacle to such movement or a traffic hazard.

We have already identified the advantages of a dedicated bus stop at Georges St., as proposed above, in both our submissions to, and meetings with WPS in the context of the Drogheda Transportation Study. In our meetings WPS agreed with the obvious coherence of our proposal, as did both ----- and -----.

To progress this proposal we would wish to meet with you and whoever else you deem appropriate to examine it further. To indicate the significance of our commitment to the provision of such a facility we would be willing to include in any such discussions the making of a contribution to any proposed works by Matthews Coach Hire Ltd. obviously the detail of this particular suggestion would be finalised in the context of a decision to provide the bus stop and the detail of the timescale involved.

We look forward to a meeting at the earliest possible time that is convenient for you and whoever else might attend.

Sincerely,

Paddy Matthews
Managing Director

Appendix B Public Realm Precedent Examples

Reference projects for public realm

Bishops Palace, Waterford **Placemaking; setting and useable space for heritage building**



Plan for new 'garden' space at Bishop's Palace, Waterford



New 'garden' space at Bishop's Palace, Waterford

This strategic project forms part of the Viking Triangle tourism area of Waterford City and is directly opposite the popular Waterford Crystal Experience site. The former Bishops Palace has been turned into a new museum for the city.

A new upper garden was developed in a former carpark to the side of the Palace. This green oasis creates a permeable public space for the city population and its many visitors to informally gather relax on the grass or varied seating options, as well as a play space for children to run between the planted mounds. The garden takes cognisance of its relationship with the historic building providing an appropriate setting for this important public building

Kilkenny Wayfinding and Signage

Accessibility, wayfinding and signage in a heritage area

The medieval core of the city means that many streets are narrow, especially footpaths. The narrow winding character opens out onto grand open spaces and small squares such as the newly completed Parade, and High Street junctions, and the Tholsel arcade, courtyard spaces and parks. There is a desire to have street furniture as minimal as possible to avoid visual clutter. This means that signage, bollards, and lamp posts are frequently unseen or can lack clarity. The use of stainless steel and light grey finishes has little visual impact, but is also hard to see not only for people with visual impairments. This is compounded when set against a backdrop of grey stone.

Signage in a heritage city such as Kilkenny has to strike a balance between being sympathetic and subtle, and being easily identifiable and understood. The development of a family of signs is based on the methodology and analysis of the information that is required, with the character of the setting. The minimal footprints derive from the narrow streetscapes, the selection of materials is derived from the use of stone and more contemporary use of stainless steels and aluminium. The colour comes from the analysis of colour in the cityscape as well as the implicit richness of quality and culture that the city enjoys. The angle and chamfer is suggestive of the K form in Kilkenny, and other logos such as the Coats of Arms of the City and Butlers, and indeed Cats have been considered.



Signage along High Street, Kilkenny City

Slips, Kilkenny.

The slips are characteristic of Kilkenny's streetscape, similar to the lanes in Drogheda. Despite the narrow stepped slips, they are engaging as routes and places. Lighting, signage, openings onto the slips, views and even unintended greenery help to make them popular and iconic.



Butter Slip, Kilkenny



Right: The Lanes, Brighton: an example of high value retail within the historic lane network of Brighton

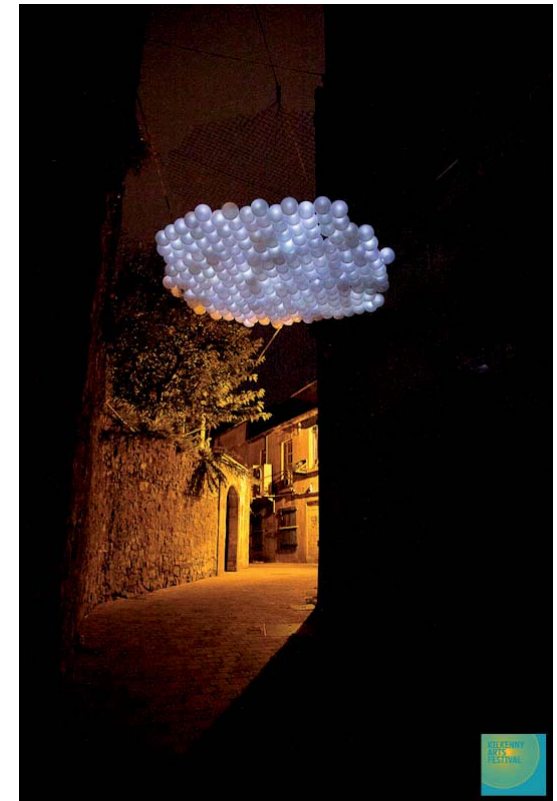


Canal Street Space Kilkenny

Lighting to animate redesigned space at base of Castle Walls and Heritage buildings



The small space is a setting for the castle and a transition space at the river crossing. Although it only has passive use it is animated by the engaging lighting scheme at night.



Temporary artwork, installed for Kilkenny Arts Festival 2012 (St. Mary's Lane)



Barking Town Square: inclusion and local skills artwork

The public art element provides the fourth elevation to the wall, conceived and developed and implemented by muf, this 7 metre high folly recreates a fragment of the imaginary lost past of Barking. The project involved a number of diverse groups in its detail design, this included students from the Theatre School, elders from the Afro-Caribbean lunch club and apprentices from the local bricklayers college

Heritage Week Event

Most Innovative Event

Event: I Am Wall

Organisers: Brendan McSherry, Louth Heritage Officer and Marcella Bannon, Drogheda Arts Centre

The aim was to bring the walls of Drogheda to life and to allow the people of Drogheda reclaim their Walled Town heritage. The people of Drogheda to come out and hold hands right around the line of the former town walls circuit (or as close to it as we could get). As we were told that red is the most noticeable colour, we asked the participants to wear red. 2,000 red 'I am Wall' T-shirts were printed and these were either collected by participants before the day of the event or were handed out on the day itself.

More than 2,000 people took part. They were on site by 11:45, on the Sunday morning. The signal to join hands was the now traditional ringing of the bells of all the churches of Drogheda at midday. Hands were held while an Air Corps helicopter we had borrowed flew over the town photographing the crowds below. The roar of the cannons on the Millmount, Drogheda's castle (originally a motte and bailey fortress) signalled the time to let the wall break apart again, at a quarter past twelve. We also had demonstrations of medieval crafts and traditional building skills.



Judges comments: This event showed great originality. Its idea and execution demonstrate how a part of our heritage that has all but vanished (only 10% of the walls remain) can be brought back to life through human spirit and creativity.



Appendix C: SEA / AA